

# SITE MASTER PLAN

## TOWN OF PELHAM EAST FONTHILL MIXED USE CENTRE

JANUARY 2015

The Planning Partnership





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# //Acknowledgements

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# 1.0 INTRODUCTION

The East Fonthill Mixed Use Centre is part of the East Fonthill Secondary Plan Area, which, in turn, is connected to the existing community of Fonthill, and specifically to its historic Downtown.

The proposed East Fonthill Mixed Use Centre is a quintessential mixed use district that will establish a healthy, sustainable and complete community. It has been planned based on the principles of Smart Growth, and includes the components for Active Transportation.

The development of the proposed East Fonthill Mixed Use Centre is an unprecedented opportunity for the Town of Pelham. It has the potential to accommodate retail and office commercial uses, major community facilities, medical facilities and an array of medium and higher density house forms, including housing for seniors.

Further, the East Fonthill Mixed Use Centre presents an opportunity to create a 'Greater Downtown Fonthill' that connects and integrates the historic Downtown Area with new development to the east. In doing so, both older and newer commercial areas can thrive by retaining their distinct but complementary characteristics.

The Town of Pelham Official Plan requires that a 'Site Master Plan' be produced for proposed developments in the Mixed Use Centre. The Plan stipulates that the Site Master Plan include: Road, Block and Land Use Plan(s), Streetscape and Open Space Plan(s), Urban Design and Architectural Control Guidelines, a Servicing Plan and an Environmental Impact Statement. This document is intended to fully articulate the first three required components. The Servicing Plan and Environmental Impact Statement are to be included under separate cover. The Site Master Plan is organized as follows:

- The Context;
- The Proposed Development;
- The Urban Design and Architectural Control Guidelines; and,
- The Public Realm Design Details.

## 1.1 THE CONTEXT

niagara region



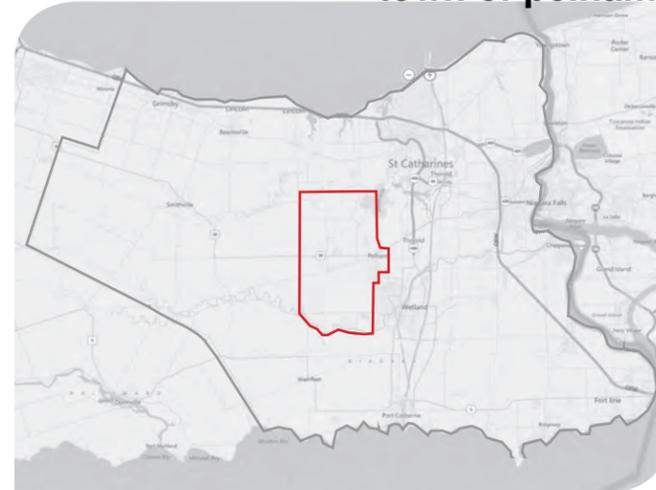
### NIAGARA REGION

Niagara Region is the upper tier body of government that oversees the planning and delivery of certain activities and functions to the various municipalities in the Region, including the Town of Pelham.

Niagara Region covers an area of 1,852 square kilometres with a population of approximately 427,421 residents in its 12 area municipalities (Niagara Region website, 2014). Niagara Region is bounded by the United States to the east, Haldimand County and Hamilton to the west, Lake Ontario to the north and Lake Erie to the south.

Land use planning at the Regional level is focused on consistency with Provincial plans and policies, defining a definable urban structure and meeting the principles of “Smart Growth”.

town of pelham



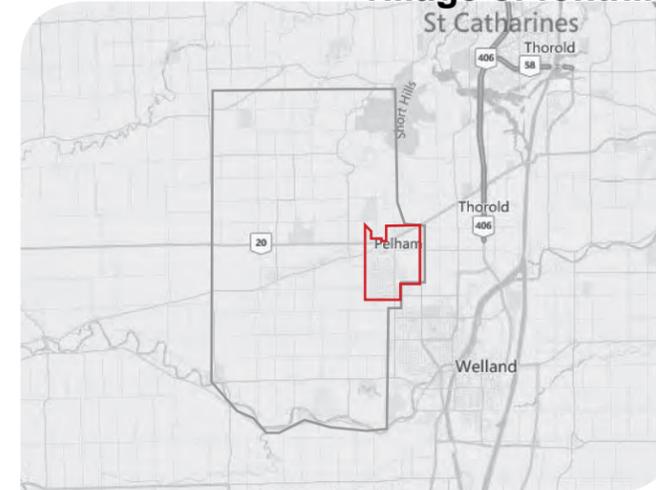
### TOWN OF PELHAM

The Town of Pelham is a local municipality located at the physical centre of the Region of Niagara. The Town of Pelham is located west of the communities of Thorold, Welland, Niagara Falls and Niagara on the Lake, east of Lincoln, north of Wainfleet, north of Port Colborne and south of St. Catherines, and about equidistant between Lake Ontario and Lake Erie.

The Town has a land area of about 125 square kilometres and a population of approximately 16,600 persons (2011 Census). The Town continues to experience moderate population growth, somewhere between 3 and 6 percent in each of the last two census periods. The Town of Pelham, aside from the primary settlement areas, is largely rural and comprised of a somewhat hilly topography. It includes components of the Niagara Escarpment.

The Town of Pelham was formerly part of Welland County. In 1970, the five villages of Fonthill, Ridgeville, Effingham, North Pelham and Fenwick were amalgamated to create the current Town of Pelham. Each village still retains its own original unique attributes that distinguish it from the other villages. The unification of the various villages that make up Pelham has given the Town a varied focus on agriculture, aggregate resources, the delivery of certain local and regional commercial or service functions and the provision of housing.

village of fonthill



### VILLAGE OF FONTHILL

The village of Fonthill is one of the two primary settlement areas within the Town of Pelham, the other being the smaller community of Fenwick located several kilometres to the south and west.

Fonthill is named after the Fonthill Kame-Delta, a prominent physiological feature created during the retreat of the Lake Erie ice lobe of the Wisconsin Glacier which covered the Niagara Peninsula. The Fonthill Kame-Delta affords wonderful views and vistas to other portions of the Region as it is the highest point of land in the Area.

Fonthill serves as the primary commercial and service centre for the Town of Pelham as well as being a bedroom community for residents who commute to work in the nearby centres of Thorold, Welland, St. Catherines and Port Colborne for employment, as well as some to even more distant employment locations such as Buffalo, Hamilton, Toronto and other centres within the Greater Toronto Area.

east fonthill secondary plan area



### EAST FONTHILL SECONDARY PLAN AREA

The East Fonthill Secondary Plan Area is the most substantial new urban development area in the Town. Approved concurrently with the Official Plan in 2014, the Secondary Plan provides policies for a comprehensively planned community structure that protects environmental features, supports the existing historic Downtown, and recognizes and responds to Regional and Provincial policy initiatives, while establishing a comfortable and attractive community.

The Secondary Plan Area will develop over time. When fully developed, the East Fonthill Secondary Plan Area is expected to include the proposed Mixed Use Centre, and four residential neighbourhoods and a highly interconnected greenlands system. It will accommodate between 6,500 and 7,500 new residents and jobs, at an average density of 50 persons and jobs combined per hectare.

## east fonthill mixed use centre



### EAST FONTHILL MIXED USE CENTRE

The proposed East Fonthill Mixed Use Centre is located in the Fonthill Settlement Area, within the East Fonthill Secondary Plan Area, southwest of the intersection of Regional Road No. 20 (Hwy 20) and Rice Road (RR54). The proposed Mixed Use Centre is approximately 19.7 hectares in size.

The proposed Mixed Use Centre has direct access to both Regional Road No. 20 (Hwy 20) to the north and Rice Road (RR54) to the east, with approximately 410 metres of frontage along Regional Road No. 20 (Hwy 20) and approximately 400 metres of frontage along Rice Road (RR54).

The proposed Mixed Use Centre is largely vacant and devoid of any structures or improvements. There are no existing structures that will be retained or incorporated as part of the future development.

The proposed Mixed Use Centre is bisected by two sub-watersheds within the jurisdiction of the Niagara Peninsula Conservation Authority (NPCA). The northern portion of the property is part of the Twelve Mile Creek Watershed and the southern portion of the site is part of the Beaverdams and Shriners Creek Watershed.

The proposed Mixed Use Centre is expected to develop as a higher density mixed use community with a retail focus and a full range of associated institutional, medical and community uses. Residential uses will also be integrated, including housing for seniors.

## 1.2 PLANNING & REGULATORY FRAMEWORK

A tremendous amount of planning has been undertaken by the Province, the Region and the Town of Pelham in recent years to ensure that future growth and development in the Town takes place in a coordinated and sustainable manner, while addressing the needs and aspirations of the community.

### 1.2.1 PROVINCIAL POLICY

Policies and strategic direction for urban growth have been established through a number of recent Provincial planning policy initiatives, which are aimed at more efficient growth management within existing, planned and future communities in the Province, including in the Town of Pelham

The following are relevant Provincial planning documents that have been reviewed and considered in the planning and design of the proposed Mixed Use Centre:

- The Planning Act;
- The Provincial Policy Statement; and,
- Places to Grow.

### THE PLANNING ACT

The Ontario Planning Act provides a set of legislative ground rules for land use planning throughout the Province and describes how land uses may be regulated and who may regulate them. The Planning Act also allows the Minister of Municipal Affairs to issue policy statements on matters related to land use planning that are of Provincial Interest.

The following requirements of Part I of the Planning Act are relevant to a coordinated approach to planning matters that cross the Town of Pelham and East Fonthill boundaries:

- *The protection of ecological systems, including natural areas, features and functions;*
- *The supply, efficient use and conservation of energy and water;*

- *The adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;*
- *The orderly development of safe and healthy communities;*
- *The adequate provision and distribution of educational, health, social, cultural and recreational facilities;*
- *The adequate provision of a full range of housing, including affordable housing;*
- *The coordination of planning activities of public bodies;*
- *The resolution of planning conflicts involving public and private interests;*
- *The appropriate location of growth and development; and,*
- *The promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians.*

It is these requirements that form the basis of the policy-led planning system throughout the Province. Flowing from the Planning Act, additional Provincial policy has been formulated which subsequently guides Regional and local planning activities.

### PLACES TO GROW: GROWTH PLAN FOR THE GREATER GOLDEN HORSESHOE

The Growth Plan for the Greater Golden Horseshoe has been prepared under the Places to Grow Act and acts as a framework for implementing the Government of Ontario's vision for building stronger, more prosperous communities through improved growth management. The Growth Plan promotes and supports mixed use and more efficient development. Section 2.1 states the following:

*...Better use of land and infrastructure can also be achieved by building more compact greenfield communities that reduce the rate at which land is consumed. Communities will need to grow at transit supportive densities, with transit-oriented street configurations. Compact urban form*

*and intensification efforts go hand-in-hand with more transit: not only do they support each other, they are all necessary. This correlation is fundamental to where and how we grow. Communities will also need to provide a mix of jobs and housing to create opportunities for people to work close to where they live...*

The proposed East Fonthill Mixed Use Centre is premised upon the establishment of a much more compact, mixed use plan than has typically been provided elsewhere within Fonthill, or the Town of Pelham in the past. Although the proposed Mixed Use Centre does contain some elements with traditional format retail uses and surface parking, it is supplemented by higher density residential forms and mixed use development which combines commercial with residential or office uses and a larger scale Community Centre in a more urban environment. The proposed Mixed Use Centre promotes a strong live-work relationship through its compact urban form and mix of uses.

Targets for a mix of population and employment which are more compact and that move towards being transit supportive are already established within the East Fonthill Secondary Plan and are implemented in the proposed Mixed Use Centre. The Growth Plan policy on Greenfield Areas requires the following to be reflected in new development on these lands:

*New development taking place in designated greenfield areas will be planned, designated, zoned and designed in a manner that –*

- *Contributes to complete communities;*
- *Create street configurations, densities, and an urban form that support walking, cycling, and the early integration and sustained viability of transit services;*
- *Provides a diverse mix of land uses, including residential and employment uses, to support vibrant neighbourhoods; and,*
- *Creates high quality public open spaces with site design and urban design standards that support opportunities for transit, walking and cycling.*

Located within a Designated Greenfield Area, the proposed Mixed Use Centre is based upon high quality urban design standards and is in conformity with the specific policies stated by the Growth Plan. The proposed Mixed Use Centre is representative of a complete community by providing a mix of uses, amenities and services and by being located in proximity to historic Downtown Fonthill, and its associated retail, administrative and cultural facilities.

## THE PROVINCIAL POLICY STATEMENT

The Provincial Policy Statement (PPS) provides policy direction on matters of Provincial interest related to land use planning and development. All decisions made by Council “shall be consistent with” the Provincial Policy Statement.

The PPS is a Province-wide policy, and is to be read in its entirety, giving consideration to the specific local circumstances. Key policy directions of the PPS are to build strong communities through the promotion of efficient land use and development patterns that support strong, livable and healthy communities, protect the environment, and facilitate economic growth.

The proposed Mixed Use Centre is consistent with the PPS, and represents an excellent example of the application of compact, transit supportive development that includes an array of land uses, all connected by a multi-modal Active Transportation System.

### 1.2.2 REGIONAL AND LOCAL POLICY

It is a requirement that all local planning policies and decisions on planning/development matters conform to Regional policy and directives. In planning and designing the proposed Mixed Use Centre, the following documents have been relied upon:

- The Niagara Regional Policy Plan (2010, as amended);
- The Niagara Region Smart Growth Principles; and,
- The Town of Pelham Official Plan (2014).

## NIAGARA REGIONAL POLICY PLAN

The East Fonthill Mixed Use Centre is a ‘Designated Greenfield Area’, which is within the Urban Area Boundary in the Regional Official Plan. The following applies to the proposed Mixed Use Centre:

*Designated Greenfield Areas will be planned as compact, complete communities by:*

- *Where permitted by scale, accommodating a range of land uses including residential, commercial, institutional, recreational, employment and other uses;*
- *Where limited by scale or configuration, making a significant contribution to the growth of the respective urban area as a complete community;*
- *Providing opportunities for integrated, mixed land uses; and,*
- *Creating street patterns that are fine grain and in grid pattern, supporting transit and active transportation within the area and to adjacent areas.*

The proposed Mixed Use Centre is comprehensive in its design, meeting the policies from the Region’s Policy Plan. It is of a large scale and by virtue of that, it will result in a community that will provide for a mix and range of residential, commercial, recreational, institutional and employment uses that will be highly integrated and make a significant contribution to the growth of Fonthill and Pelham in general, as a complete community. At the same time, it provides for an appropriate street pattern that will be linked with other areas of the Fonthill community and encourage Active Transportation.

## NIAGARA REGION SMART GROWTH PRINCIPLES

The following highlights some of the Region of Niagara’s Smart Growth Principles that have been appropriately considered in the design of the proposed Mixed Use Centre:

- *Create a mix of land uses;*
- *Promote compact built form;*
- *Offer a range of housing opportunities and choices;*
- *Produce walkable neighbourhoods and communities;*
- *Foster attractive communities and a sense of place;*
- *Provide a variety of transportation choices;*
- *Make development predictable and cost effective; and,*
- *Encourage community stakeholders collaboration. (Niagara Region Website, 2013)*

The proposed Mixed Use Centre incorporates many of the Region’s Smart Growth Principles. The blocks and buildings are designed at a pedestrian friendly scale and oriented to facilitate walkability within the site. The scale is compact incorporating a mix of uses, including a range of residential, recreation and local services, and assisting in the creation of a complete community. Active Transportation is supported by a connected system of streets, sidewalks, paths and trails to provide options for more active modes of travel. Existing natural features are preserved and enhanced.

## TOWN OF PELHAM OFFICIAL PLAN (2014)

The Official Plan of the Town of Pelham is a primary planning document that directs the actions of the Town and shapes growth and development. The Official Plan establishes a vision for the future land use structure of the Town. According to Regional growth projections, it is anticipated that the population of the Town will increase by approximately 7,600 people over the next 20 years. Under the Official Plan, a majority of this growth is directed to East Fonthill and, to a lesser extent, Fenwick.

The Official Plan for the Town specifies new policies for the proposed Mixed Use Centre through the East Fonthill Secondary Plan. Fonthill is recognized under the Urban Area Designation, Urban Living Area, but the East Fonthill Secondary Plan Area is designated part of the ‘Secondary Plan

Areas’ and ‘Greenfield Overlay’ on Schedule A1 of the Official Plan. The Greenfield Overlay area policies establish:

*a strategy for the future development of Greenfield Areas in the Town by describing these areas as being either “high priority” or “low priority” areas for development. Lands characterized in the following policies as “high priority” are lands that the Town views as being essential to the Town’s Settlement Area Strategy and as such, are anticipated to accommodate new development within the first 10 years that this Plan is in effect.*

The land use structure of the Official Plan is divided into three main land use categories: urban area designations, rural area designations and natural heritage designations. The intent of the new Official Plan is organized into eight categories in section A2, with goals and objectives are provided for each of the eight categories:

- *Natural environment;*
- *Growth and settlement;*
- *Urban character;*
- *Economy;*
- *Infrastructure;*
- *Natural resources;*
- *Cultural heritage; and,*
- *Community improvement.*

The proposed Mixed Use Centre development meets the intent of the applicable goals and objectives of the Official Plan with respect to natural environment, growth and settlement, urban character, economy and infrastructure.

## Natural Environment

The Natural Environment section identifies the priorities to preserve, maintain, enhance and restore the health and integrity of the ecosystem. Existing environmental features within the proposed Mixed Use Centre, as well as within the southern portion of the East Fonthill Secondary Plan Area, have been preserved as part of the Greenland System through the Environmental Protection One and Two designations. In some instances, these features have been integrated with stormwater ponds and open spaces, which provides for additional enhancements and linkages of the Greenlands System.

## Growth and Settlement

The Growth and Settlement section identifies that new development is directed to Urban Areas, specifically in and in proximity to the historic Downtown. The proposed Mixed Use Centre is located in close proximity to Downtown Fonthill and proposes a mix of uses and a range of housing for the community. The Town has identified specific urban areas for future development, which includes Greenfield Areas in order to meet the growth targets allocated by the Region. Since the proposed Mixed Use Centre is within the Greenfield Areas designation, this site will accommodate a minimum of 50 people and jobs per hectare.

## Urban Character

The Urban Character section seeks to protect and enhance the character of existing Urban Areas. Key objectives identified under urban character that pertain to the Proposed Mixed Use Centre are:

*To maintain and enhance the character and stability of existing and well-established residential neighbourhoods by ensuring that development and redevelopment is compatible with the scale and density of existing development*

*To ensure that new development areas are integrated into the fabric of the existing community in conformity with approved Secondary Plans*

*To encourage the development of neighbourhoods which are compact, pedestrian-friendly and provide a mix of housing types, community facilities, small-scale commercial centres and public open spaces*

*To provide community facilities that are safe, visible and accessible to residents in each neighbourhood*

Since the proposed Mixed Use Centre is currently vacant, there is an opportunity to define and establish an appropriate character for this new development. The Site Master Plan establishes a development concept for the proposed Mixed Use Centre that relies upon strong elements of building, site design and landscaping to ensure that future development makes a positive contribution to the urban character of Fonthill by being compatible with existing development and appropriately integrated within the existing community.

## Economy

The Economy section identifies, as a goal, opportunities for economic development and the creation of jobs. The key policies that pertain to the proposed Mixed Use Centre are:

*To promote active transportation and the use of open space to encourage tourism and to attract residents seeking an active recreational community as a lifestyle or retirement choice*

*To encourage opportunities for employment within Regional context including a broad range of commercial and service facilities*

The proposed Mixed Use Centre incorporates a mix of uses including commercial, a Community Centre, medical centre and senior's residence, which will provide diverse employment opportunities. Active Transportation will also attract residents seeking an active lifestyle or retirement community promoting East Fonthill as a retirement choice, in proximity to an array of medical, retail and community facilities.

## Infrastructure

The Infrastructure section includes as its goal that the necessary infrastructure including, sanitary sewers, water distribution and stormwater management facilities and roads, be provided to serve the needs of the community.

The proposed Mixed Use Centre is located within the Urban Area and will require full services. The proposed Mixed Use Centre will enable development in a pattern that is compact, pedestrian and cyclist friendly, consist of a range of uses, mix of housing types and contribute to the overall revitalization efforts of the neighbouring historic Downtown Fonthill. The proposed layout includes two stormwater management ponds to manage water and runoff.

The positive impacts of the proposed Mixed Use Centre will be to draw people from the other parts of Pelham to use the proposed facilities such as the Community Centre. The proposed Mixed Use Centre development will accommodate and draw a number of new residents and jobs to East Fonthill, contributing to the Town's density targets forecasted by the Official Plan. Representative of a complete community, the proposed Mixed Use Centre will reduce land consumption, sprawl and servicing costs. It will provide much needed retail, medical and community facilities.

### 1.2.3 THE EAST FONTHILL SECONDARY PLAN

As part of the Official Plan Review, a Secondary Plan for East Fonthill was prepared and integrated into the new Town of Pelham Official Plan. The purpose of the East Fonthill Secondary Plan is to provide specific policies for a community structure that is comprehensively planned on the basis of Smart Growth principles to protect environmental features, support the historic Downtown, integrate with the existing Fonthill community and establish a new community with a range of housing, commercial and community facilities.

The proposed Mixed Use Centre is a fundamental component of the larger East Fonthill Community. It is the location for retail and office commercial uses, institutional and community facilities, and an array of higher density residential uses, including housing for seniors. The following timeline outlines major milestones in the East Fonthill Secondary Plan process.

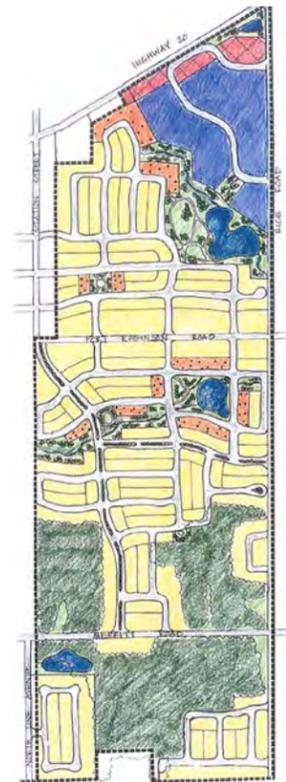
The East Fonthill Secondary Plan, and including the proposed Mixed Use Centre, has a long history beginning in 2001, and ending with Ontario Municipal Board approval in 2014. The following provides a summary of the evolution of the East Fonthill Secondary Plan Area, including the Mixed Use Centre.

2001

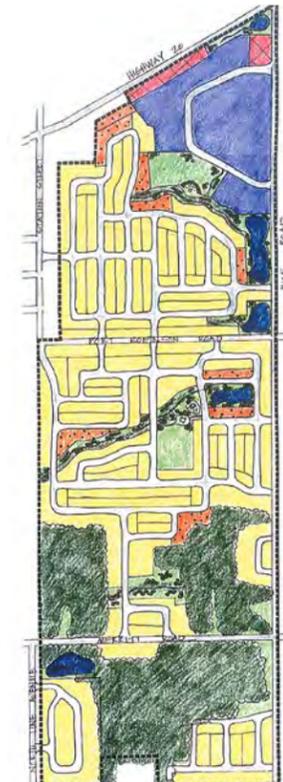
### OPTIONS WERE DEVELOPED AT A DESIGN WORKSHOP

A design workshop was held in mid-2001. Through the design workshops, opportunities and constraints were explored and options and concepts were developed to illustrate the potential development of the East Fonthill Secondary Plan Area. The options explored various distributions for high and low density residential uses, and lands for commercial and employment related uses. In addition, different road configurations and connections, open space networks and connections, were considered.

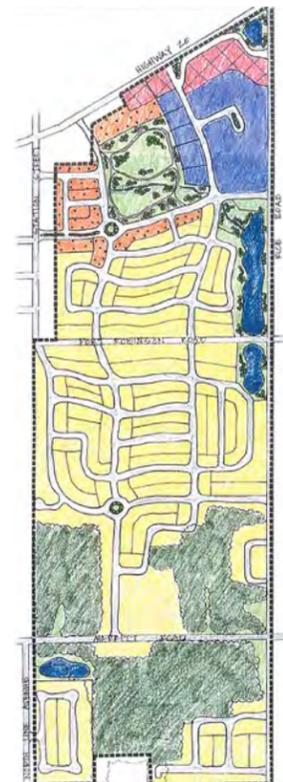
Option 1



Option 2



Option 3



2003

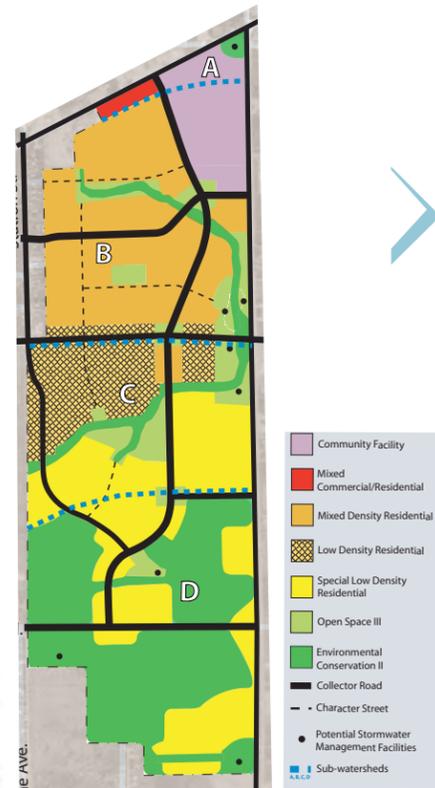
AN ANALYSIS OF THE OPTIONS LED TO A PREFERRED PLAN

Residential density is concentrated to the north with decreasing density towards the south. Commercial uses and business park located adjacent to RR20 (Hwy 20), with the business park in the vicinity of RR20 (Hwy 20) and Rice Road (RR54). The street pattern in Option 3 was most preferred due to connectivity to the Town Square. Park space distribution is linked to the natural features, and open spaces are located to ensure that most new residents are served by a park within 5-minute walk.

Preferred Plan



Land Use Plan



2004

LAND USE PLAN AND DEMONSTRATION PLAN.

A land use schedule was generated based on the Preferred Plan and a detailed Demonstration Plan was created based on the Land Use Schedule.

Demonstration Plan



2005/2006

MAJOR CHANGES IN PROVINCIAL POLICY

In 2005 and 2006 the Province enacted a number of key policy directives - changes to the Planning Act, a new Provincial Policy Statement, Places to Grow and the Greenbelt Plan. These new directives required the Plan to be re-evaluated and re-confirmed.

Structure Plan



Land Use Plan



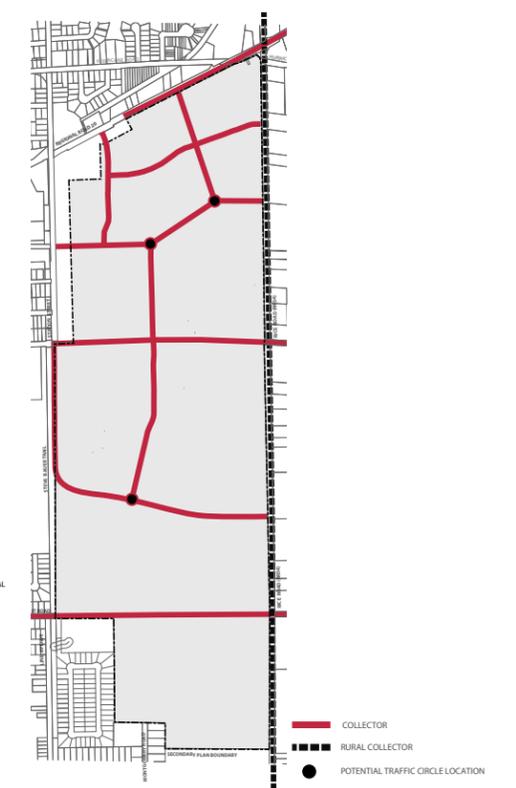
2014

APPROVED SECONDARY PLAN

The East Fonthill Secondary Plan and Urban Design Guidelines were further refined and ultimately approved by the OMB in 2014. The East Fonthill Secondary Plan included:

- Community Structure;
- Plan Objectives;
- Design and Phasing Policies;
- Land Use Designations;
- Residential Neighbourhoods;
- Commercial/Employment Centre;
- Greenlands System;
- Environment and Groundwater Management; and,
- Implementation and Administration.

Transportation



2014

## DEMONSTRATION PLANS & URBAN DESIGN GUIDELINES

As part of the approved East Fonthill Secondary Plan, as part of an appendix to the Official Plan, Urban Design Guidelines were also created. These guidelines include a detailed Demonstration Plan for Land Use and a Demonstration Plan for Active Transportation.

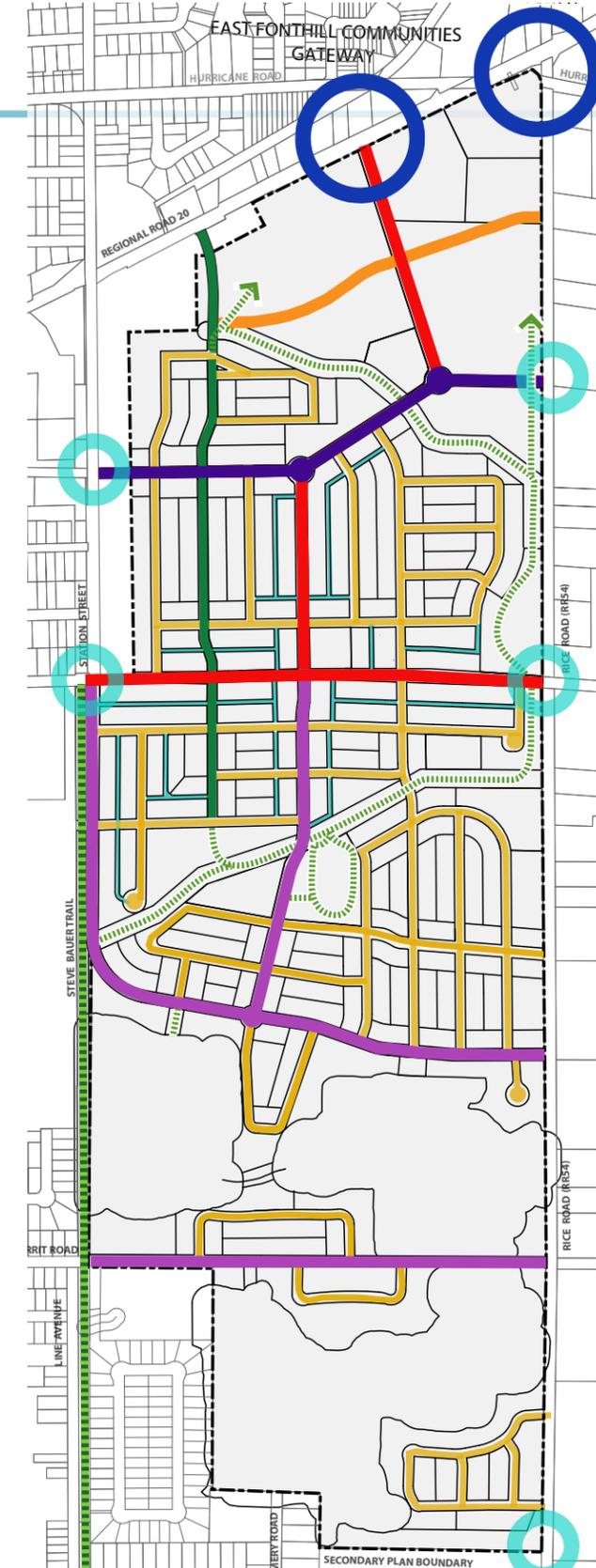


### Design Guidelines for the Private Realm

- All Development
- Residential Development
- Urban Highway Commercial and Mixed Use
- Development
- Institutional Development

### Demonstration Plan for Land Use

- COMMERCIAL / EMPLOYMENT**
- COMMERCIAL / EMPLOYMENT
  - MIXED USE
- RESIDENTIAL**
- HIGH / MEDIUM DENSITY
  - MEDIUM DENSITY
  - LOW DENSITY
  - MEDIUM DENSITY CAP END
  - LOW DENSITY CAP END
- OPEN SPACE**
- STORMWATER MANAGEMENT POND
  - NEIGHBOURHOOD PARKS
  - PARKETTES LINKAGES
- NATURAL HERITAGE**
- ENVIRONMENTAL PROTECTION ONE
  - WATERCOURSES



### Design Guidelines for Public Realm

- Roads
- Traffic Circles
- Streetscape Elements
- Gateways
- Trail Network
- Open Space
- Stormwater Management Facilities

### Demonstration Plan for Active Transportation

- MAJOR GATEWAY
- MINOR GATEWAY

### EAST FONTHILL STREET NETWORK

- CEREMONIAL STREET
- COLLECTOR URBAN 'MAIN STREET'
  - SIDEWALKS ON BOTH SIDES OF STREET
  - BIKE LANES (BOTH DIRECTIONS)
  - LANEWAY HOMES INTERFACE
- COLLECTOR NEIGHBOURHOOD 'MAIN STREET'
  - SIDEWALKS ON BOTH SIDES OF STREET
- MIXED USE CENTRE COLLECTOR
- LOCAL GREENWAY
  - SIDEWALK ON BOTH SIDES OF STREET
  - BIKE LANES (BOTH DIRECTIONS)
- LOCAL URBAN
  - SIDEWALK ON BOTH SIDES OF STREET
- LANEWAYS

- TRAILS**
- EXISTING STEVE BAUER TRAIL
  - PROPOSED OFF ROAD TRAILS

## EAST FONTHILL POLICY FRAMEWORK

Section B1.7.3.1 of the East Fonthill Secondary Plan provides a number of General Development Objectives. Many of the objectives are reflected and in fact, exemplified in the proposed Mixed Use Centre including:

- *Well-designed compact urban form inclusive of a mix of uses;*
- *An appropriate pedestrian-oriented scale that is also walkable and bicycle friendly;*
- *Sense of identity and continuity within the community;*
- *Complete community;*
- *Intensification of the community to achieve 50 persons and jobs per hectare;*
- *Supportive land use and community structure to the historic Downtown;*
- *Significant retail commercial development;*
- *Connected Greenlands System;*
- *Roads designed at a pedestrian scale and also bicycle friendly;*
- *Land use pattern to support alternative transportation options;*
- *Hierarchical road system;*
- *Development with full servicing facilities; and,*
- *Phased development.*

The proposed Mixed Use Centre is located within the Commercial/Employment Centre component of the Secondary Plan, where two land use designations apply as follows:

- *The Commercial/Employment Centre includes lands designated EF-Mixed Use and EF-Urban Highway Commercial on Schedule 'A5' - Land Use Plan. On those lands, it is the intent of the Town to promote a variety of retail commercial facilities, office and institutional uses, residential uses and community uses in a form that mixes those uses within buildings and on the site. Stand-alone and mixed use buildings will be permitted; and,*
- *It is also the intent of the Town to:*
  - *Ensure a vibrant, inviting and appealing atmosphere on these lands that will attract residents and new businesses, encourage walking and cycling and resonate with visitors a positive image of Fonthill;*
  - *Facilitate the development of a new major shopping destination;*
  - *Ensure that development within the Centre is comprehensively planned to ensure that the impacts of servicing, access, loading and parking can be adequately addressed;*
  - *Ensure that the required minimum density of 50 persons and jobs combined per gross hectare is achieved within the Commercial/Employment Centre; and,*

- *Ensure that leading-edge telecommunication services are in place through discussions with telecommunications providers to attract knowledge based industries and support the technological advancement and growth of existing businesses.*

Consistent with the general intent of the Commercial/Employment Centre designation in the Secondary Plan, the proposed Mixed Use Centre promotes a variety of facilities and uses, mixed both on site and within individual buildings. Further, the proposed Mixed Use Centre will facilitate development of a major new shopping destination and provide an opportunity for creation of a vibrant and appealing atmosphere to attract new residents and businesses to Fonthill, and to encourage walking and cycling.

The Site Master Plan process required under the Secondary Plan has, and will continue to assist in ensuring that development is comprehensively planned and incorporates proper servicing, access, screening of loading and parking areas. Lastly, the proposed Mixed Use Centre will result in the potential for approximately 120,000 square metres of gross floor area on approximately 20 hectares of land, achieving an overall Site Floor Space Index of 0.6 (gross). As such, these lands have the potential to meet or exceed the expectation of achieving a minimum combined density of 50 persons and jobs per gross hectare.

More specifically, the proposed Mixed Use Centre has been designed, in conformity with the policies of Section B1.7.8.4 EF-Mixed Use designation of the East Fonthill Secondary Plan, which states:

*Buildings within the EF-Urban Highway Commercial designation are encouraged to accommodate an array of uses. This mixture of uses is encouraged not just within the designation in general, but also on individual development sites, and within individual buildings. High activity uses that animate the streetscape and encourage foot traffic, like retail uses, are encouraged at-grade and/or along the Arterial and Collector Road frontages, with uses such as offices, recreational, cultural and residential uses on second floors and above and/or behind the street-related façade. Office and other appropriate employment uses shall be encouraged above grade in mixed-use buildings located throughout the EF-Urban Highway Commercial designation, and in single-use buildings that are located off Arterial or Collector Road frontages.*

## 1.2.4 SUPPORTING & IMPLEMENTING DOCUMENTS

The East Fonthill Mixed Use Centre requires substantially more detailed planning, design engineering, environmental and archaeological work to be carried out to facilitate development. To date, a number of studies have been carried out, including:

- A Traffic Impact Study;
- A Preliminary Stormwater Management Plan;
- A Master Environmental Servicing Plan;
- An Environmental Impact Statement; and,
- A Stage I through IV Archaeological Assessment.

In addition to those supporting documents, a comprehensive Planning Justification Report has been prepared in support of an implementing Official Plan Amendment and Zoning By-law Amendment that are specifically intended to facilitate the development of the proposed Mixed Use Centre.

### TRAFFIC IMPACT STUDY

A Traffic Impact Study was prepared by Paradigm Transportation Solutions Limited which summarizes the Transportation Study for Phase 1 of the proposed East Fonthill Secondary Plan (which is known in this document, as the proposed Mixed Use Centre). The study findings indicate that additional traffic is expected to occur as a result of the development and assesses the impact of traffic on the surrounding road network. The findings, conclusions and recommendations of the study have been presented in detail in the submitted East Fonthill Secondary Plan Phase 1 Traffic Impact Study, April 2013.

### PRELIMINARY STORMWATER MANAGEMENT PLAN

A Preliminary Stormwater Management Plan was prepared by Upper Canada Consultants (UCC) in April 2014. The study established criteria for the management of stormwater from the proposed Mixed Use Centre; determined the impact of development on the peak flow of stormwater; investigated alternatives for controlling the quality of stormwater; and, determined preliminary lands necessary for stormwater management as needed to support the draft plan of subdivision.

### MASTER ENVIRONMENTAL SERVING PLAN (MESP)

A Master Environmental Servicing Plan (MESP) was prepared by Upper Canada Consultants (UCC) in January 2014 for the proposed Mixed Use Centre. It identified preliminary locations for pole lines, conduits, watermains, sewer and other underground and over ground utilities and structures as necessary.

### ENVIRONMENTAL IMPACT STATEMENT (EIS)

An Environmental Impact Statement (EIS) was prepared by Beacon Environmental in support of the proposed Draft Plan of Subdivision within the East Fonthill Secondary Plan Area. It was undertaken following the requirements of the Regional Municipality of Niagara Environmental Impact Study Guidelines.

The EIS determined that the proposed Draft Plan of Subdivision will not negatively impact any natural features or functions. The proposed Draft Plan of Subdivision for the proposed Mixed Use Centre is in conformity with the Official Plan and Natural Heritage Systems policies of the Town of Pelham, and Niagara Region, and the East Fonthill Secondary Plan. These policies also address the Province's Natural Heritage policies under the Provincial Policy Statement (PPS 2014).

The proposed Mixed Use Centre review of impacts to watercourses and fish habitat, as well as proposed mitigation measures, were prepared by Lisa Campbell and Associates (LCA) and submitted in conjunction with the EIS.

### ARCHAEOLOGICAL ASSESSMENT

A Stage 1 Archeological Assessment was prepared by Heritage Consultants Inc. for the East Fonthill Secondary Plan Area in July 2007. This Stage 1 Assessment recommended that a Stage 2 Assessment be completed prior to development.

As part of the development review for the Town owned lands in East Fonthill (south of RR20 (Hwy 20), and east of Rice Road (RR54), Golder Associates prepared a Stage 2 Archeological Assessment in July 2014, which in turn recommended a scoped Stage 3 Assessment based on two (2) high value potential locations being identified. The Stage 3 Archeological Assessment by Golder Associates

in August 2014, cleared one location of interest, but recommended a Stage 4 scoped Assessment due to the number of artifacts discovered on the second location.

Golder Associates completed a Stage 4 Assessment in October 2014, and recommended the site clear of significant archeological value. All reports have been submitted to the Ministry of Culture for confirmation and release of cultural significance.

### PLANNING JUSTIFICATION REPORT

A Planning Justification Report was prepared by The Planning Partnership in April 2014. The purpose was to review the background relating to the general location and context of the proposed Mixed Use Centre, the nature of the anticipated development and to provide a planning rationale in support of the applications to amend the Official Plan and Zoning By-law.

### OFFICIAL PLAN AMENDMENT

The East Fonthill Secondary Plan was approved by the Ontario Municipal Board in 2014. The proposed Official Plan Amendment will amend the Plan in order to facilitate the development of the proposed Mixed Use Centre.

The Official Plan Amendment is relatively simple and straightforward. It will amend the portion of lands designated EF-Mixed Use to EF-Urban Highway Commercial, such that the entirety of the Mixed Use Centre lands are designated EF-Urban Highway Commercial.

## ZONING BY-LAW AMENDMENT

Flowing directly from the Official Plan Amendment, the East Fonthill Secondary Plan, and the companion Design Guidelines, an Implementing Zoning By-law (ZBL) was developed to establish permissions for the array of anticipated land uses, to confirm the built form parameters and to articulate the building-street edge relationship within the Mixed Use Centre.

A summary of the key details of the ZBL is as follows:

### Mixed-Use 1 (EF-MU1) Zone

- This Zone is expected to accommodate the primary retail facilities within the proposed Mixed Use Centre. Permitted uses are focused on a full array of retail commercial businesses, offices, hotels and tourist accommodations, conference and convention centres, cultural, recreational and entertainment uses, and public and private institutional uses.
- Development regulations indicate that retail stores, restaurants and personal services shall be required at-grade in all buildings, with a number of specific exceptions. There is also a maximum of two drive-through facilities permitted within this Zone, and this is the only zone where these facilities are permitted.
- The MU-1 Zone may accommodate a maximum total Gross Floor Area of 30,000 square metres for all permitted uses. Building heights are regulated generally as a minimum of 6 metres (2 storeys) and a maximum of 35 metres (10 storeys). Build-within zones are also established to ensure an appropriate relationship between the main front wall of any building, and the adjacent public streets.

### Mixed-Use 2 (EF-MU2) Zone

- This Zone is expected to accommodate office space and residential land uses. The array of permitted uses includes commercial, professional, medical and/or government offices, retail commercial uses, hotels and tourist accommodations, conference and convention centres, cultural, recreational and entertainment uses, public and private institutional uses, townhouses and apartment buildings, housing for seniors and/or special needs housing and child care facilities.
- Building heights are regulated generally as a minimum of 6 metres (2 storeys) and a maximum of 35 metres (10 storeys). Build-within zones are also established to ensure an appropriate relationship between the main front wall of any building, and the adjacent public streets.

### Mixed-Use 3 (EF-MU3) Zone

- This zone is expected to accommodate a Multi-Use Recreational Facility, as well as an array of higher density house forms. Permitted uses also include a Multi-Use Recreational Facility, including cultural, recreational and entertainment uses, commercial, professional, medical and/or government offices, retail commercial uses, hotels and tourist accommodations, conference and convention centres, public and private institutional uses, townhouses and apartment buildings, housing for seniors and/or special needs housing and child care facilities.
- Building heights are regulated generally as a minimum of 6 metres (2 storeys) and a maximum of 35 metres (10 storeys). Build-within zones are also established to ensure an appropriate relationship between the main front wall of any building, and the adjacent public streets.

### Open Space (EF-OS) Zone

- This Zone permits the establishment of a major urban square, and a public park adjacent to the existing stream corridor. Permitted uses include parks and urban squares designed for the recreational enjoyment of the community, trails for cyclists and pedestrians, public art installations and small scale retail and commercial uses, including restaurants that serve the recreational function of the park/urban square.
- Development regulations include opportunities for naturalization and enhancements to adjacent EF Environmental Protection Zones (EF-EP). It is anticipated that parks and urban squares shall include hard and/or soft-scape improvements, including opportunities for portable food services, athletic activities, seating, water features, children's play features and communal gatherings/event space. Buildings within this zone shall be a maximum height of 4.0 metres (1-storey).

### Environmental Protection (EF-EP) Zone

- This Zone protects the existing stream that is on the property. Permitted uses are limited to conservation uses, flood and erosion control works and trails for cyclist and pedestrians, including bridges.
- Within this Zone, no person shall erect any building or structure, except structures for conservation purposes, structures for flood and erosion control or recreational trails and bridges authorized by the Conservation Authority.

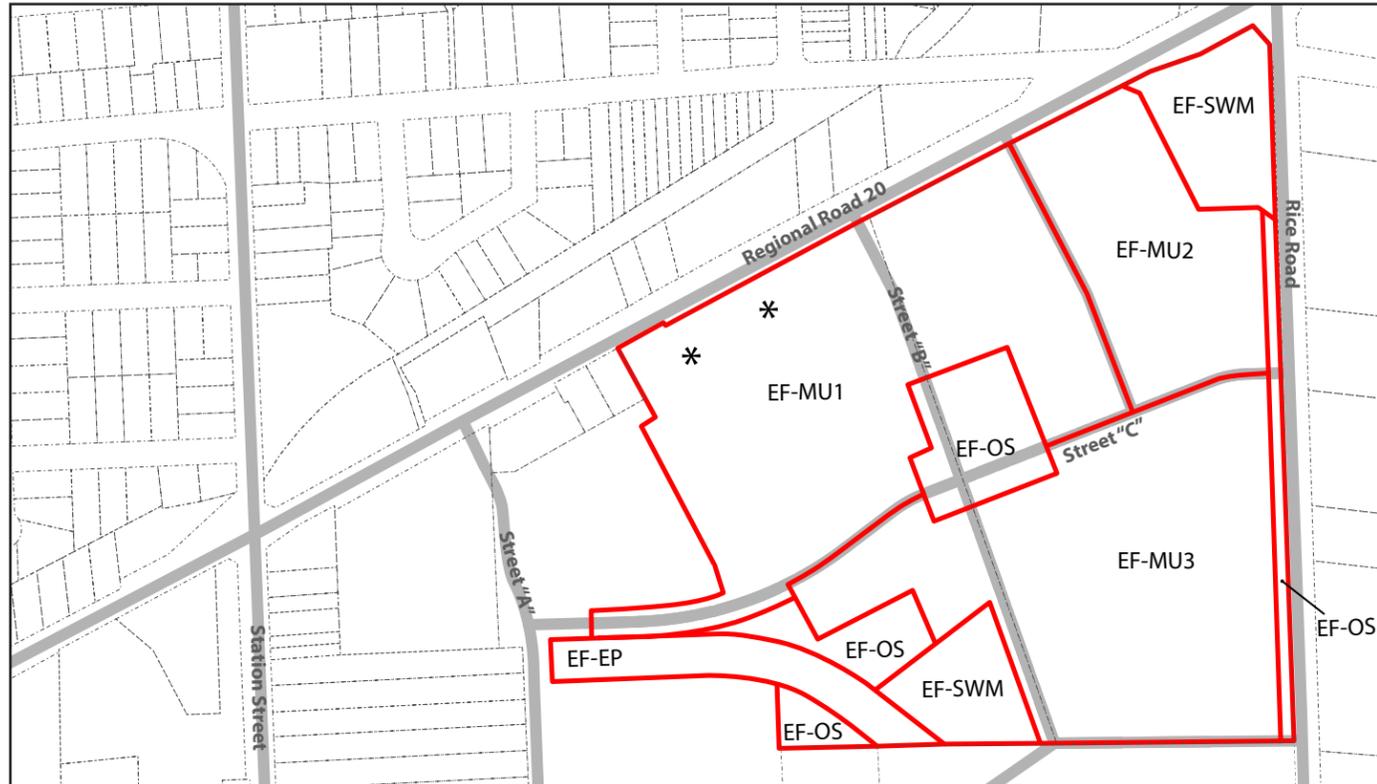
### Stormwater Management (EF-SWM) Zone

- This Zone accommodates required stormwater management facilities. Permitted uses include facilities and features that are intended to manage the flow of stormwater from adjacent development sites, water features and associated landscaping to ensure the attractiveness and safety of the stormwater management facility, trails for cyclists and pedestrians and gateway features and/or public art installations.
- Development regulations within this Zone identify that stormwater management facilities may include opportunities for naturalization and enhancements to EF-EP and EF-OS Zones.

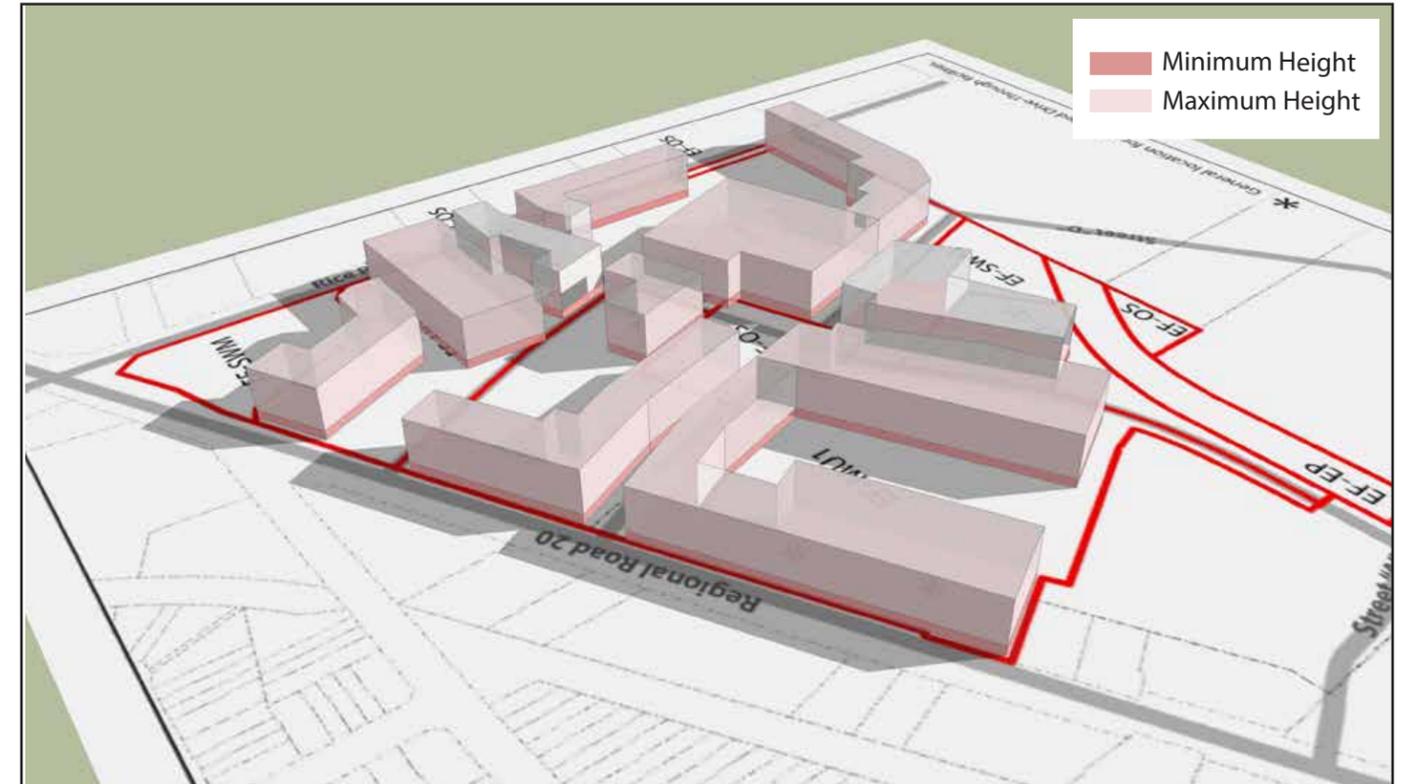
### Parking

- Parking requirements in the ZBL require that all development shall include parking for vehicles and bicycles located at-grade and/or located within a structure. All development shall also include appropriate spaces for visitors parking, parking for persons with disabilities, and/or parking for parents with young children, wherever appropriate. Minimum and maximum parking requirements are provided for each land use.

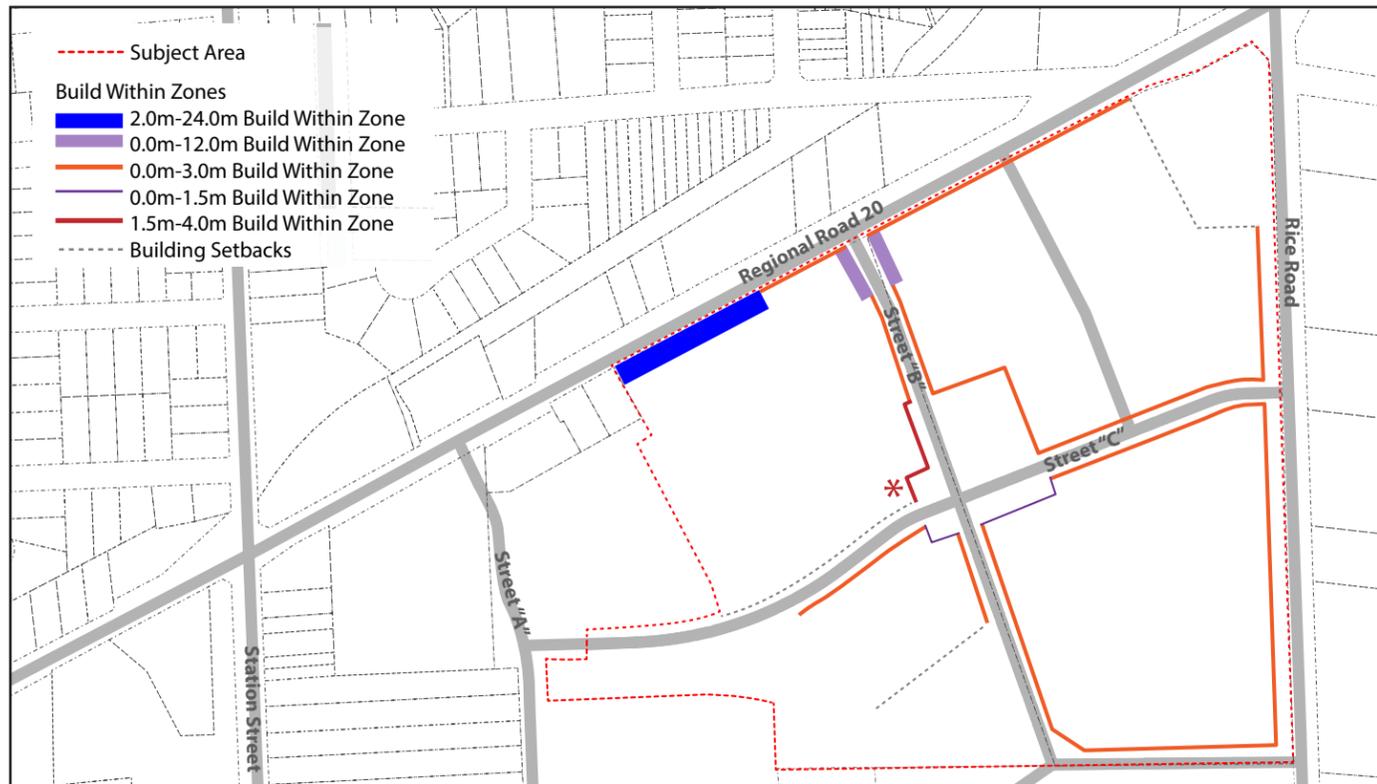
## ZBL SCHEDULE 1



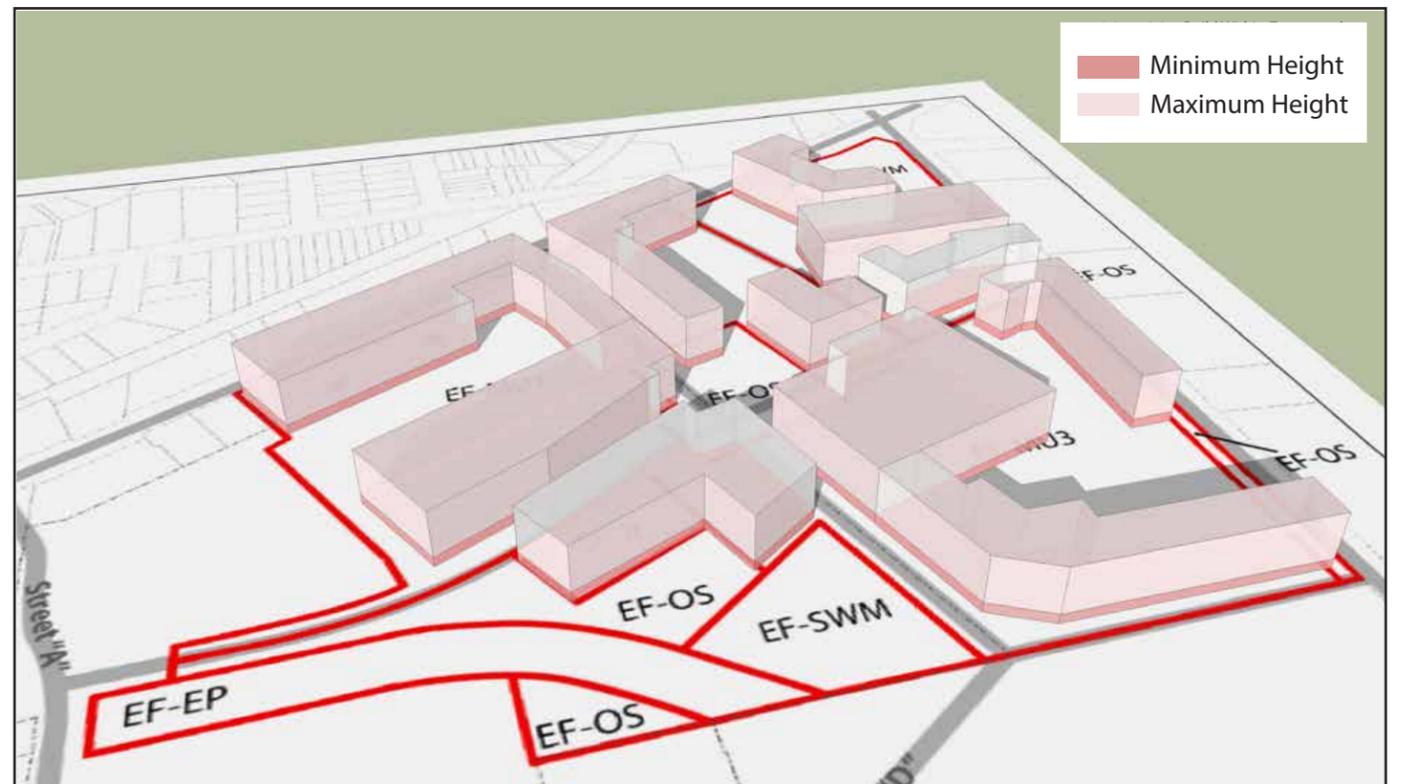
## ZBL SCHEDULE 2



## ZBL SCHEDULE 4



## ZBL SCHEDULE 3



## 2.0 THE PROPOSED DEVELOPMENT

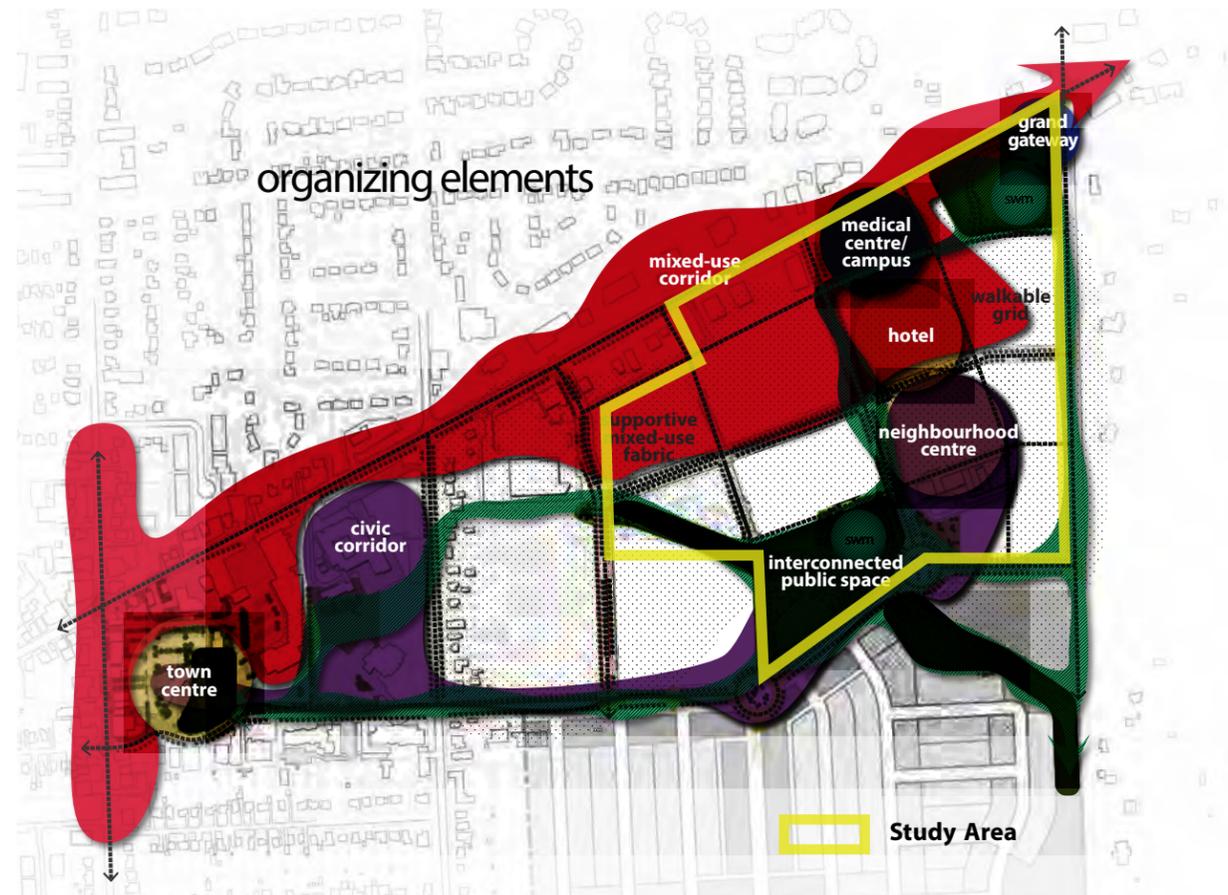
### 2.1 A GREATER DOWNTOWN FONTHILL

Building upon the “Key Downtown Strategies” contemplated in the Downtown Master Plan for Fenwick and Fonthill (November 2014), the concept of a “Greater Downtown Fonthill” is fundamental to the design and development of the proposed East Fonthill Mixed Use Centre, and all of those crucial connecting linkages that will create and support the evolving Fonthill community. Strategy Eight of the Downtown Master Plan for Fenwick and Fonthill states:

*“The proposed commercial and mixed-use area in the East Fonthill Secondary Plan presents an opportunity to create a ‘Greater Downtown Fonthill’ strategy that connects and integrates the historic downtown area with the new development to the east. In doing so, both older and newer areas can thrive by retaining their distinct*

*but complementary built characteristics. The Greater Downtown Fonthill strategy is based on:*

- *Ensuring strong and seamless physical connection through existing streets, open spaces and trails;*
- *Reinforcing visual connections with continuity in street-oriented built form and consistent streetscaping, as well as views to gateways, public art and other landmarks across both areas;*
- *Ensuring new use and developments reinforce the distinct qualities of the old and new areas, and not compete with the fine grained and small-scaled character of uses in the historic downtown area; and,*
- *Protecting RR20 (Hwy 20) to serve as the east-west ‘mixed-use corridor’, while maintaining the extension of Pelham Town Square to Rice Road (RR54) as the ‘civic corridor’.*



### 2.2 THE DEVELOPMENT CONCEPT PLAN

The Development Concept Plan is considered a demonstration of how the policies of the Official Plan result in comprehensive planning within the proposed East Fonthill Mixed Use Centre. The Concept Plan is non-statutory, and as such there is considered to be a degree of inherent flexibility in how they are interpreted and, ultimately, applied.

The Development Concept Plan depicts a demonstration of the built form for the East Fonthill Mixed Use Centre, and identifies the mixture of land uses anticipated by the zoning by-law. The Plan shows the potential long-term build out of the proposed Mixed Use Centre that is consistent with the Design Guidelines set out in this Site Master Plan. The Development Concept Plan also shows the potential long-term development of the East Fonthill Mixed Use Centre’s street network, trails, and connections and depicts the key components of the open space system for the East Fonthill Mixed Use Centre, including parks, urban squares, gateways and stormwater management facilities.

The Development Concept Plan is to be used for guidance in implementing the policies of the Official Plan through subsequent development approval processes. It is expected that if development applications are considered by the Town to be “generally consistent” with the Development Concept Plan, than some components of the complete application may be deemed to be fulfilled. If development applications are considered by the Town to not be “generally consistent” with the Development Concept Plan, than a more detailed master planning exercise will be required to facilitate subsequent approval processes.

# DEVELOPMENT CONCEPT PLAN



## 2.3 DESCRIPTION OF THE DEVELOPMENT CONCEPT

The proposed Mixed Use Centre has been designed within the context of the approved East Fonthill Secondary Plan, and in recognition of the broader development and planning objectives for the Mixed Use Centre as articulated by the Town with general regard to principles of Smart Growth, good planning and high quality urban design.

The Development Concept Plan for the Mixed Use Centre envisions creation of pedestrian oriented and scaled buildings characterized by higher order land uses – including mixed use, denser residential forms and community-scale commercial facilities. The proposed Mixed Use Centre is also intended to be enhanced by strong urban design, landscaping and public realm elements that will establish an identifiable sense of place and contribute to the quality of life in the community.

The following sections provide a summary of the proposed Development Concept for the proposed East Fonthill Mixed Use Centre.

### 2.3.1 DEVELOPMENT BLOCKS, LAND USE AND BUILT FORM

The development blocks are configured to create street frontages that frame and provide activity and interest along the street edges in the development.

The Development Concept is designed to create smaller scale blocks suited to pedestrians. Although there are also slightly larger blocks, mid-block connections compensate by providing for a more permeable street system.

East Fonthill generally and the proposed Mixed Use Centre more specifically are serviced by a comprehensive bicycle network and multi-use trails. The Development Concept encourages Active Transportation and provides for opportunities to connect to the adjacent neighbourhoods and to the larger region-wide bicycle and trails network. As development proceeds, and activity and densities increase, there may be opportunities in the future to implement transit service.

The development blocks are configured to create street frontages that frame and provide activity and interest along the street edges in the development.

The Development Concept for the proposed Mixed Use Centre is based upon the intent to establish a desirable and viable mixed use precinct within the Fonthill community that contains a broad number and variety of commercial, residential, institutional, community, and open space and environmental land uses, all of which are conducive to the creation of a vibrant, livable and complete community.

The built form proposed, provides for a diverse mix of housing types through the introduction of townhouse units and mid-rise forms of housing.

The number of dwelling units generated range from 350 to 550 units. Mixed use buildings with a combination of residential and commercial are proposed. Commercial uses are proposed at-grade with residential on the upper floors. The massing of these buildings along the public streets will be oriented to step back from the street providing room for sidewalks and boulevards for pedestrian areas.

The proposed Mixed Use Centre Development Concept is in keeping with the goals and objectives of the new Official Plan in accommodating a range of housing, community facilities and a mix of uses in proximity to historic downtown Fonthill. New development is integrated with and attempts to enhance the existing character of the surrounding community.

The proposed placement and orientation of the buildings will define the character and function of the streetscape. The Development Concept locates retail/commercial along the arterial roads and residential uses along the interior of the site. There are several entrances to the buildings proposed in the development improving accessibility throughout the Mixed Use Centre.

### 2.3.2 STREET HIERARCHY

The proposed Mixed Use Centre incorporates a street hierarchy, which includes a range of street types that respond to different functions and aesthetic character.

Significant emphasis is placed on the greening of the proposed streets with trees and plantings for an environment that is conducive to a more attractive and walkable community with a sense of place. The street trees will contribute to providing tree canopy throughout the Mixed Use Centre. A Ceremonial Route connects the Mixed Use Centre to the historic Downtown to create interest and unite these two areas. Roundabouts and woonerfs add further interest and highlight the importance of these routes.

### 2.3.3 TRAILS AND CONNECTIONS

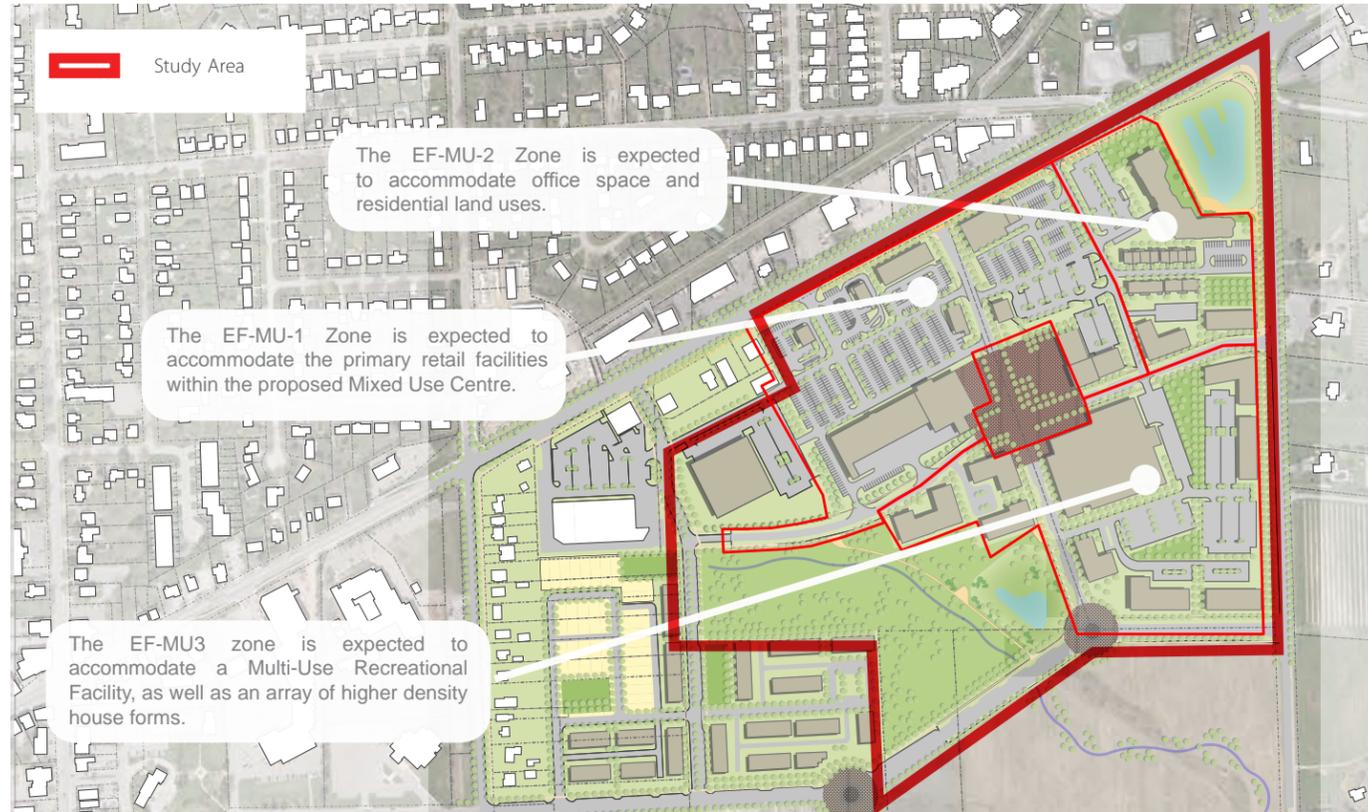
The Development Concept incorporates various key opportunities for linkages and flow of pedestrian and cyclist traffic throughout the Mixed Use Centre and externally to other parts of the Town. The design facilitates Active Transportation through improved accessibility and walkability through interconnected pathways, trails and streets.

### 2.3.4 OPEN SPACE SYSTEM

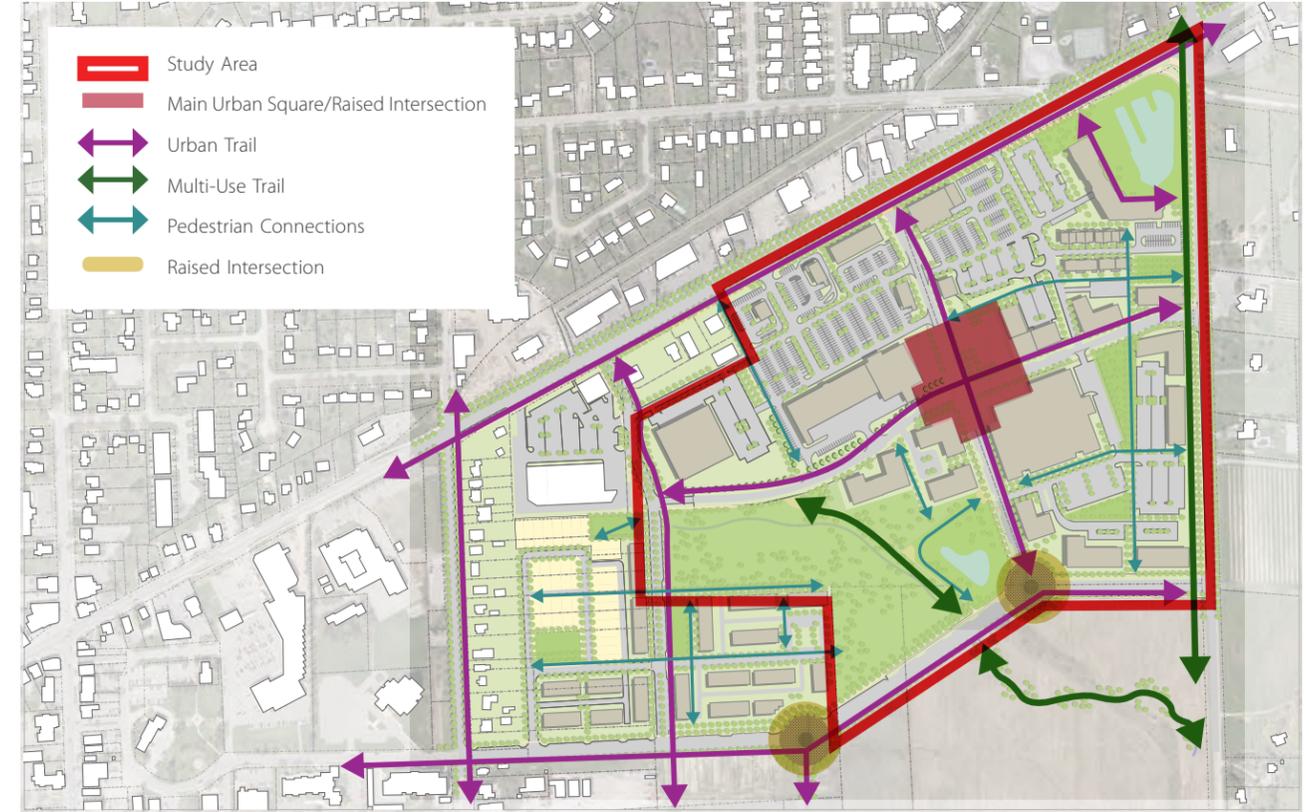
A high quality open space system within the proposed Mixed Use Centre is a crucial component in creating a more livable, complete community. The open space system is enhanced through more greening, landscaping and the incorporation of public amenities. These elements include street trees, parks, urban squares, and stormwater management facilities.

The Development Concept for the Mixed Use Centre includes street trees, parks and urban squares throughout. The public open spaces proposed are crucial to extending and linking the tree canopy in the Town. A number of public squares are proposed, which contribute to the public realm of the development. These public squares serve as key linkages/termini to overall the Public Open Space system. They also serve as meeting and gathering areas for the community, creating a sense of place.

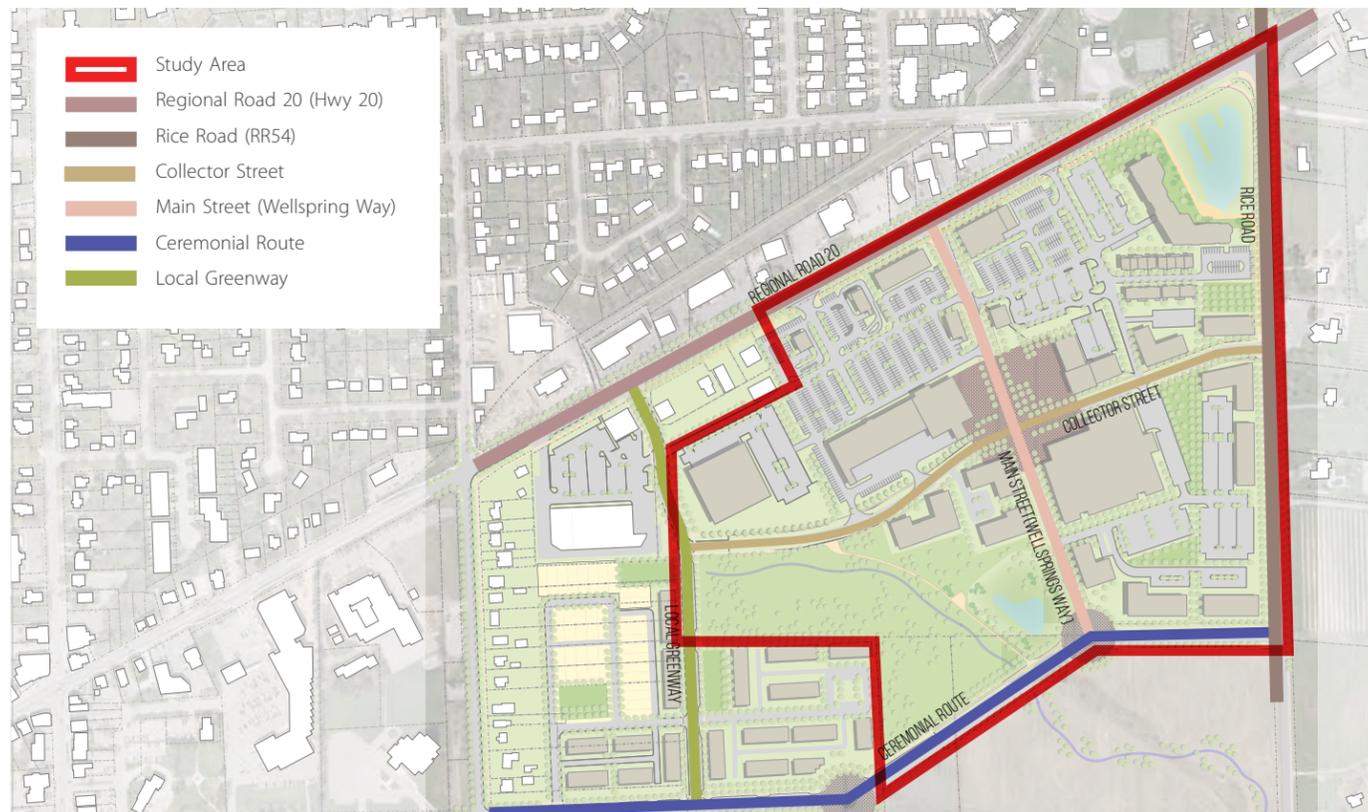
## DEVELOPMENT BLOCKS, LAND USE AND BUILT FORM



## TRAILS AND CONNECTIONS



## STREET HIERARCHY



## OPEN SPACE SYSTEM



## 3.0 THE URBAN DESIGN & ARCHITECTURAL CONTROL GUIDELINES



Linked Green spaces



Mixed Uses



Walkable Communities

The purpose of the Design Guidelines is to promote an appropriate built form within the proposed East Fonthill Mixed Use Centre that achieves a community with well-designed and high quality built form and public realm. Providing clear Design Guidelines also ensures a sense of identity is created through the design of built form, streetscape, and open spaces. The overarching design principles are to:

- Accommodate an array of retail, office, institutional and community facilities that will serve the broader Town of Pelham residents;
- Provide opportunities for a range and mix of higher-density housing types;
- Integrate a linked greenlands system that includes a variety of public parks, environmental features and stormwater management facilities;
- Protect and preserve existing environmental features from any negative impacts associated with new and adjacent development;
- Integrate a modified grid pattern of roads to maximize connectivity and continuity both within the community and with the existing broader Fonthill community, particularly the existing Town Centre;
- Design a variety of streetscapes that are compatible with adjacent land uses; and,
- Ensure a human scaled environment that is reflective of the anticipated built form within the community.

The policies of the Official Plan, and the East Fonthill Secondary Plan more specifically, require that all new development be “generally consistent” with these Design Guidelines and, as such, there is considered to be a degree of inherent flexibility in how these Design Guidelines are both interpreted and ultimately, applied.

These Design Guidelines will function as a statutory requirement for successive approval processes that are required to facilitate the development of the proposed Mixed Use Centre, particularly the zoning by-law, Draft Plans of Subdivision and Site Plans. This document will guide developers, builders and the Town to ensure that the expectation of high quality is both understood and achieved.

It is strongly encouraged that all Site Plan applications be designed with the advice of qualified professionals to minimize possible drawing revisions due to the application of these Design Guidelines. In instances where the Design Guidelines conflict with other Department comments or Town policies, standards, and/or requirements, the Director of Community Planning & Development will determine which policy/standard/regulation is paramount.

These Design Guidelines are divided into three parts: Smart Growth, Built Form and Public Realm. Smart Growth will encompass environmental sustainability, Crime Prevention Through Environmental Design (CPTED), and healthy communities. The Public Realm will encompass streetscape, parks and open space, off-road trails, and stormwater management facilities, while the Built Form will include housing forms and mixed use developments developed primarily by the private sector.

### 3.1 GENERAL GUIDELINES

The state-of-the-art building design includes and fully incorporates the principles of Smart Growth. Inherent to those principles are the concepts of environmental sustainability, CPTED and healthy community design. All of these concepts are to be considered and incorporated into the design and development of the proposed East Fonthill Mixed Use Centre.

#### 3.1.1 ENVIRONMENTAL SUSTAINABILITY

Sustainable development is development that meets the needs of the present without compromising the ability of future generations to meet their needs. As identified in the Regional Municipality of Niagara Model Urban Design Guidelines (2005), environmental sustainability is a key design concept, and has important urban design implications.

Sustainable design can be applied to both the private and public realm, and encompasses streets, parks, and buildings. As part of the strategy to achieve environmental sustainability in both public and private development, proposed new development shall be consistent with to the following guidelines:

- Paved areas (such as parking lots, driveways, and streets) should be minimized wherever possible and should be constructed of permeable materials and porous surfaces in order to reduce the volume of runoff into the storm drainage system;
- Wherever possible, vegetative or grassy swale drains should be considered and incorporated into the road design for non-urban road cross sections and in surface parking areas in order to maximize water infiltration and cleansing runoff;

- Unless impractical, new street trees and landscaping should be of native plant materials and grown naturally within the Region;
- Light coloured materials should be used for parking lots; the use of blacktop should be avoided, when possible;
- Streetscaping and building elements should be chosen for their whole life cycle costing and aesthetic value, and determined by suitability, durability, ease of maintenance, and cost effectiveness;
- Native, local, indigenous, and post-consumer recycled building materials should be used wherever practical;
- Well lit bicycle storage facilities should be provided at public parks and public buildings in order to encourage alternative modes of transport;
- Wherever possible, new development should seek LEED certification (or equivalent), ensuring higher performance standards in environmental responsibility and energy consumption;
- Green roofs should be considered to minimize water runoff and improve building insulation; and,
- Wherever possible, buildings should be designed with large south-facing glazed surfaces in order to maximize solar orientation.



Example of vegetative or grassy swale drain



Example of adaptive reuse to preserve historic structures and neighbourhood



Create safe communities

### 3.1.2 CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (CPTED)

CPTED is a multi-disciplinary approach to deterring criminal behavior that relies upon the environment to influence a potential offender's choices, prior to committing a criminal act. It is based on the notion that instead of a reactionary disjointed response to crime, that typically results in increased fortification and fear of crime, a set of scientifically proven principles can be employed to better develop a site, which will positively influence the likelihood of proper and appropriate behavior on that site.

CPTED has been proven to work, and to this end, the application of these principles on all new development will over time, reduce the propensity of crime and fear of crime throughout the Town. All new development within the proposed East Fonthill Mixed Use Centre shall be consistent with the principles and provisions of CPTED, as follows:

#### PRINCIPLES

**Natural Surveillance** - Maximizing visibility and the opportunity for observation through the placement and design of physical and social features. This includes the placement of gathering spaces/points of interest, building orientation, lighting, windows, entrances/exits, parking lots, walkways, security stations, fencing, landscaping, vegetation, signage, as well as any other physical obstructions. This principle helps create the perception of risk to an offender.

**Natural Access Control** - A logical and organized design to restrict, encourage, and safely channel the movement of people and vehicles into, out of, and within a site in a controlled manner. Natural Access Control should be coupled with Natural Surveillance techniques. This principle helps create the perception of control over the offender, as well as more easily identify those who venture into areas they should not be in.

**Territorial Reinforcement** - Defined property lines and clear distinctions between public, semi-private, and private spaces through physical or visual design can create a sphere of territorial influence that can be perceived by, and may deter potential offenders. Territorial reinforcement can be created using among other things, landscaping, pavement designs, gateway treatments, signs and fences. This principle helps create a sense of proprietorship by the rightful user and helps more easily identify those who venture into areas they should not be in.

**Maintenance** - Well-maintained buildings and grounds inform potential offenders that 'Someone is Home'. It also reinforces the space's intended use and purpose. This principle helps create a sense of occupancy to the offender, as well as guardianship and proprietorship by the rightful user.

**Logical Fortification** - Fortifying buildings or property by the use of materials and/or equipment. The logical use of fortification should never conflict with any other CPTED principle. This quasi-principle although not widely held as a base principle of CPTED is nonetheless important to deter or even prosecute criminal activities. This quasi-principle helps create a sense of obstacle or inconvenience to the offender.

#### TOWN OF PELHAM-CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN (TOP-CPTED) GUIDELINES

- All sites must employ and enhance both natural and designed surveillance opportunities. These opportunities should, by design, be focused on places of criminal interest in addition to entry/exit points;
- Commercial buildings should provide maximum window coverage on all front and side first storey façades (facing higher traffic areas) to promote surveillance of both interior and exterior views of the



Example of bicycle storage rack



Example of bicycle storage on street



Example of promotion of alternative modes of transport

building(s). As a general rule, glazing should allow an interior visibility of a building to a minimum 1.8 metre depth;

- Sites should be designed to limit visual barriers of sightlines from the street to building exit/entry points;
- Walkways/pathways to and from buildings should be located in areas supported by lighting and surveillance, and should not be located near ambush points;
- All site lighting must adhere to the Site Lighting Standards of the Town of Pelham, which include design requirements for:
  - Fixture (luminaire);
  - Mounting;
  - Light Source (Lamp);
  - Illumination Levels; and,
  - Photometric Plan Submission;
- Points of building access should be located in areas of high visibility and the site should be designed to enhance attention to these areas;
- Points of access to a building should be limited in number and centralized where possible;
- Sites that utilize or design private or semi-private space that abuts public space shall employ territorial markers;
- Sites that utilize or design private space that abuts semi-private space shall employ territorial markers;
- Bicycle storage shall be located in illuminated areas that have high visibility;
- All fencing, other than privacy fencing, shall employ approximately 20 percent openings portioned throughout;

- Sites should be designed to facilitate easy maintenance;
- Use of defensive (thorny) vegetation is permitted in areas requiring defense measures, such as along interior fence lines, under accessible windows, or bordering outdoor storage areas, and may also be permitted in limited supply as an interior barrier to private spaces; and,
- Decorative ground or non-building façade lighting should be placed behind the feature (away from the street) to illuminate and reduce any potential hiding or stealth movement areas.

### 3.1.3 HEALTHY COMMUNITIES

Healthy communities provide opportunities for walking and exercise in a safe and comfortable environment. The guidelines provided in this Section are intended to improve pedestrian and cyclist comfort and enjoyment, and are intended to work in conjunction with the other guidelines provided in this document. All development within the proposed East Fonthill Mixed Use Centre shall be consistent with the following healthy communities guidelines.

### STREET CONNECTIVITY STANDARDS

Street networks and off-road paths should always:

- Provide the maximum choice for how people will make trips;
- Take full account of the kinds of movement a development will generate;
- Make clear connections to existing routes and facilities;
- Intersections should be frequent, with street blocks decreasing in size as density increases; and,

- Sidewalks, bike lanes and multi-use paths should connect to street networks, major destinations and transport nodes.

### SIDEWALK AMENITIES

- All streets must have sidewalks on each side that are least 1.6 metres wide;
- A variety of street trees that are hardy, resilient and low maintenance should be planted at regular intervals (as specified in the Public Realm Guidelines) adjacent to all streets; and,
- Transit shelters and other street furniture should be provided throughout the proposed East Fonthill Mixed Use Centre. Other street furniture includes benches, waste receptacles, newspaper outlets, community information boards, water fountains, public washrooms, bicycle parking, and bicycle sharing system components.

### CYCLING AMENITIES

- A connected and destination-oriented bikeway network should be provided throughout the proposed East Fonthill Mixed-Use Centre, including a variety of on- and off-street bikeway facilities that provide an appropriate degree of separation from motorized traffic, given the speed and volume of traffic on the street. These on-street bikeway facilities may include (but are not limited to) bicycle lanes, cycle tracks, sharrows, signed routes, bicycle boulevards and multi-use paths on the boulevard;
- The connected bikeway network will seamlessly connect the East Fonthill Mixed Use Centre to the rest of Fonthill, and to the rest of the Town of Pelham and beyond;

## INTERSECTIONS

All intersections in the proposed East Fonthill Mixed Use Centre should be designed to increase the visibility of cyclists and pedestrians, give them priority, reduce crossing distance and provide adequate crossing time. Intersection design elements may include, but are not limited to:

- Pavement treatments and markings for pedestrian crossings (e.g. brick paving, zebra/ladder markings);
- Curb cuts/ramps;
- Raised crosswalk;
- Curb extension/bulb out;
- Centre Median or refuge island;
- Pedestrian scramble;
- Bicycle box;
- Conflict zone markings for bicycles (e.g. coloured lane, skip lines, chevrons, sharrow);
- Audible pedestrian crossing signals;
- Countdown signals;
- Leading pedestrian and/or bicycle signals (advance walk/bike signal);
- Pedestrian and/or bicycle actuated signals;
- Right-turn on red light prohibitions; and,
- Mid-block signalized crossings.

## TRAFFIC CALMING

In the proposed East Fonthill Mixed Use Centre will implement development traffic calming on public streets by using:

- Minimum traffic lane widths;
- Minimum number of traffic lanes in the roadway;
- Pedestrian-priority streets, shared streets, or woonerfs;
- Speed humps;
- Bollards (with shields);
- Channelization islands (raised islands that force traffic to turn in a particular direction);
- Chicane (curb bulges or planters or alternating sides, forcing motorists to slow down);
- Choker (raised islands in parking zones that narrow a roadway);
- Curb extension, planter, or centreline traffic island that narrows traffic lanes;
- Horizontal shift (a lane centreline that curves or shifts);
- Rumble or warning strip;
- Semi-diverter or partial closure (restricts entry and limits traffic flow at intersections);
- Signal timing to reduce traffic speeds;
- Radar trailer that shows drivers their current speed and the posted speed limit; and,
- Traffic circles or roundabouts.

While increasing comfort and safety for pedestrians, the design of traffic calming elements should not create undue hazards or obstacles for cyclists.

## LIGHTING

- All streets within the proposed East Fonthill Mixed Use Centre must have an average luminance of 10 lux, with a minimum of 5 lux; and,
- Pedestrian-level street lamps of 4.6 metres in height or less, spaced apart no more than 30 metres must be provided on all streets. Street lamps must provide uniform illumination.

## WAYFINDING

- A wayfinding system should be implemented within the proposed East Fonthill Mixed Use Centre, and on a community-wide basis to allow residents and visitors to determine their location; identify key destinations (parks, transit stations, community and cultural facilities, shopping centres, off road trails); and develop a plan to take them from their location to desired destination by walking or cycling. The wayfinding system may include maps, directional signs or other elements, and should be useful and easy to understand.



Traffic Calming



Traffic Calming and Lighting



Pedestrian Connections and Parking



Parking

## AUTOMOBILE PARKING

- Reductions in parking requirements should be given to:
  - Buildings and other facilities within 400 metres of a transit stop; and,
  - Apartments/condominiums offering car share parking spaces (with each car share space equivalent to 10 regular spaces);
- On-street parking should be included on all streets except where inappropriate for technical or safety reasons;
- Efficient use of parking should be promoted by identifying systems for sharing parking spaces by two or more user groups at different times of the day or week (for example, office staff use during weekdays and restaurant clientele in the evenings and on weekends), and by providing preferential parking for carpool vehicles;
- Where available, economic incentives should be identified and utilized to provide structured parking, rather than surface parking; and,
- Where surface parking is provided, it should be designed to minimize negative aesthetic and environmental impacts. This can be achieved by locating the parking lot way from the street frontage and by incorporating the following into the parking lot design:
  - Tree planting;
  - Landscaping;
  - Stormwater management;
  - Porous/permeable surfaces;
  - Light-coloured materials (rather than black asphalt); and,
  - Pedestrian access and circulation.



Bicycling Parking

## BICYCLE PARKING

- All new developments within the proposed East Fonthill Mixed Use Centre shall provide adequate illuminated bicycle parking. It shall be located in a safe and accessible locations; and,
- The minimum bicycle parking standards are outlined in the following table:

Minimum Bicycle Parking Standards, by Use and Type

Use	Minimum Space by Bicycle Parking Type	
	Occupant/ Employee*	Visitor **
Multi-unit Residential	0.7/unit	0.8/unit
Retail, Services & Community Facilities	0.1/100 m <sup>2</sup>	3 + 0.25/100 m <sup>2</sup>
General Office	0.15/100 m <sup>2</sup>	3 + 0.25/100 m <sup>2</sup>
Medical Office	0.15/100 m <sup>2</sup>	3 + 0.1/100 m <sup>2</sup>
Hospital	0.06/100 m <sup>2</sup>	3 + 0.06/100 m <sup>2</sup>
Elementary/ Secondary School	0.06/100 m <sup>2</sup>	3 + 0.06/100 m <sup>2</sup>
Post-Secondary School	0.06/100 m <sup>2</sup>	3 + 0.2/100 m <sup>2</sup>
Other non-residential (e.g. Industrial)	0.06/100 m <sup>2</sup>	0.1/100 m <sup>2</sup>
High-order Transit Station	<i>Complete a bicycle parking demand estimate for the station, for example using boardings, alightings and local bicycle mode share data.</i>	

\*Occupant/Employee ("long-term") parking refers to secure, enclosed bicycle storage that is locked, weather protected and easily accessible to residents and/or workers. Signage indicating the location and information on use of these parking facilities should be provided.

\*\*Visitor ("short-term") parking refers to outdoor, covered/uncovered bicycle racks.

## 3.1.4 PUBLIC ART

A rich and varied selection of public artworks throughout a town enhances the sense of place and belonging which defines a healthy community environment. Public art provides the opportunity to communicate the story of Pelham, its history, cultural heritage and natural heritage.

The development of a public art strategy would provide the framework to initiate/implement a public art plan, including how to administer and fund public art as well as how to select, place, maintain and preserve art works. Public Art Opportunities include:

### INDEPENDENT ART

Independent or stand-alone art primarily takes the form of sculptures that are created in a studio or fabricator's workshop and then shipped to site and installed in selected locations.

### SEMI-INTEGRATED ART

Semi-integrated artworks are often multi-component, composed of independent elements, as well as elements that are integrated into the landscape such as the Gateway Pond where Town identity signage is proposed to be located.

### INTEGRATED ART

Integrated artworks are conceived of and executed as a component of the architectural or landscape architectural design of a project. Integrated artworks may be constructed by the artist through a design/build contract or can be competitively bid as a component of the larger capital project.



Public Art and street amenities

## 3.2 BUILT FORM GUIDELINES

The Built Form Guidelines provide appropriate standards or benchmarks for housing forms, commercial, institutional and community buildings, and mixed use development within the proposed East Fonthill Mixed Use Centre. These guidelines help to guide and shape new development in order to reinforce the design and development objectives of the Town. At the same time, they provide flexibility within certain parameters to encourage distinction, and variety, in addition to creative architectural responses. Where developments do not conform to the guidelines but propose alternative standards, they should be assessed by the Town to ensure that the intent and spirit of the guidelines are met with respect to their primary objectives.

It is important that all buildings in the proposed East Fonthill Mixed Use Centre maintain a small town feel and are designed to reflect a sense of community. For that reason, attention must be given to the scale and detail of every building. All development within the proposed East Fonthill Mixed Use Centre shall be consistent with these Built Form Guidelines.

### 3.2.1 ORIENTATION & PLACEMENT

The orientation and placement of buildings along the street help to reinforce the public realm by enhancing the pedestrian environment through creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings, providing the appropriate levels of animation and use. Key guidelines for the orientation and placement of buildings are as follows:

#### GENERAL GUIDELINES FOR ALL BUILDINGS

- All buildings should orient to and address the street with clearly defined entry points that directly access the sidewalk;
- Buildings should be placed at or close to the street edge, consistent with adjacent building setbacks, and build-within zones established in the implementing zoning by-law;
- The arrangement of buildings along the street should provide views into the site and facilitate pedestrian movement;
- Wherever possible, create outdoor areas which connect the different buildings/uses around passive seating areas; and,
- Spill-out commercial activity such as outdoor cafés/patios should be encouraged.

#### PROMINENT VISUAL SITES

Sites located at gateways, at the terminus of view corridors and adjacent to parks or urban squares have greater visual prominence, and are identified as Prominent Visual Sites. While gateway sites have frontages onto two streets and frame intersections, view terminus sites can be highly visible from great distances. Those sites can help to enhance visual connectivity and orientation. Accordingly, they have a greater

civic obligation and should be designed to give good form and prominence to the sites they occupy.

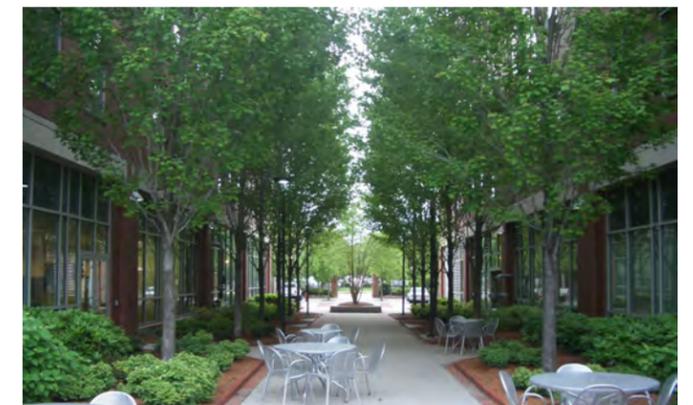
- Siting and massing of buildings should reinforce prominent visual site and corner conditions;
- Building massing, architectural details, glazing, façade articulation, fenestration and changes in building height may be used to frame prominent visual sites and corners;
- As new developments on prominent visual sites can shape the image and character of an area, the highest possible standards in design and material quality should be encouraged;
- All buildings facing onto parks and other open spaces will need to be architecturally upgraded with added fenestration, articulated walls and lighting. Where this is not feasible, other means for screening large, poorly articulated walls shall be used (i.e. landscaping with mature trees);
- New development on terminus sites should align design features to the view axis which, in addition to tall architectural elements, can include aligned entries or portico openings;
- New developments on all corner sites should be oriented to both street frontages. Entrances should be clearly defined;
- Hard and soft landscaping should be considered and coordinated with building design to emphasize prominent visual sites;
- Taller building elements should be sited, where possible, to terminate vistas, reinforce important intersections, and signal the location of key focal points within the community;
- The taller element should maintain an appropriate scale, (e.g. height, pitch, roof, etc.) in relation to its building and other neighbouring structures;



Mixed Use Corner



Pedestrian Scale



Views into site



- The architecture of the taller element should reflect that of neighbouring structures in massing, architectural design, and materials;
- The most substantial façade treatments shall occur facing the higher order street;
- Driveway access shall not be located close to the intersection;
- Wherever possible, create outdoor areas that connect to adjacent open space features; and,
- Where elevations flank onto open space they shall be of consistent materials, window styles and detailing as per the front elevation.



## USE-SPECIFIC

In addition to those general guidelines, the following use-specific guidelines shall be considered:

### Retail and Mixed Use

- Where retail entrances are accessed from parking areas within the block and have a façade on the street, both façades must be treated with equal attention to architectural details and functions;
- Where the main entrance is not on the street, transparent and active storefronts must be incorporated into street edge façades;
- Encroachments into the right of way may include balconies, signage or and architectural details which do not obstruct the public sidewalk;
- Built form may be of individual “free standing” building or of a block design format with 3 to 10 businesses sited side by side; and,



- Where individual residential units are accessed on the ground floor, entrances should be elevated a minimum of 3 steps to provide privacy.

### Community/Institutional

- Community/Institutional buildings should be placed to enhanced their visibility and provide sense of place; and,
- Institutional buildings shall front on adjacent public streets and be located close to the street to reinforce the streetwall, to create a visually dominant feature in the community and to define intersections.

### High and Medium Density Residential

- The siting, massing, and façade design of residential buildings shall be coordinated on a block-by-block basis;
- Residential buildings should be oriented towards the street with front doors and windows facing the street;
- Townhouse units located at intersections shall be designed to ensure that both street frontages comprise active living spaces to facilitate appropriate openings and upgraded architectural features. Common elements adopted for exposed façades include:
  - Wall changes of plane with a minimum projection of 0.3 metres from the wall face;
  - Generous number of openings which will also serve to allow more light into the rooms;
  - Bay and/or box-out window features, projected at a minimum of 0.3 metres from the wall face;
  - Porches or balconies;

- Symmetrical and harmonious arrangement of materials, openings and design elements;
- Townhouses shall, whenever possible, have rear lane garages to reduce visual impact of garages on the street;
- Townhouse built form will be limited to a maximum of 8 units, with 6 units preferred. Where 8 units are proposed, individual unit widths should not exceed 6.5 metres;
- Where garages are provided in the front yard they should be paired to allow for more substantial front yard green space. Garages shall not protrude beyond the main front wall or front entry of the unit;
- Apartment buildings and their surrounding landscaping shall be designed to help define the street edge; and,
- To act as an attribute to the street, apartment buildings shall be oriented to front, face and feature the public road. A substantial portion of the building should front the public road at a minimum setback.

### 3.2.2 HEIGHT & MASSING

Building heights throughout the proposed East Fonthill Mixed Use Centre are between 2 and 10-storeys. Building heights play an important role in the impact on the character and quality of the street experience. The height and mass of the building can ensure visual continuity, maintain pedestrian scale, animation of the street and design quality. Guidelines for building massing include:

#### GENERAL GUIDELINES FOR ALL BUILDINGS

- Buildings taller than 2-storeys should provide a minimum 2.0 metre stepback at the front and exterior side building wall above the second storey. Buildings taller than 4-storeys should provide an additional stepback; or other appropriate architectural expression;
- Interior buildings higher than 5 storeys should employ scaling details;
- Buildings should maintain an appropriate scale (e.g., stepped façade, pitch roof, setbacks) in relation to adjacent uses; and,
- Large scale buildings shall be broken up using fenestration, change of materials and textures, scaling, and differing surface depths.

#### ROOF LINES

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs of buildings are also seen from other buildings of equal or greater height. Roof design shall be consistent with the following guidelines:

- The expression of the building top and roof, should be clearly distinguished from the rest of the building

through treatments such as stepbacks, change in materials, cornices lines, and overhangs;

- Rooftop mechanical equipment should be integrated with the architectural treatment of roofs and/or fully screened from view;
- Green roofs should be encouraged to provide for amenity and to meet sustainable objectives;
- If it is not feasible to accommodate second storey ancillary uses above commercial uses, then characteristics of a second storey are encouraged in the architecture of retail buildings through the use of a variety of roof lines and roof elements such as dormers, gables, parapets, etc; and,
- Roof top patios should be encouraged on buildings 4-storeys or greater.

#### USE-SPECIFIC

In addition to those general guidelines, the following use-specific guidelines shall be considered:

##### Retail Storefronts and Mixed Use

Well-proportioned and designed storefronts can contribute positively to the pedestrian environment by providing animation and visual interest at the sidewalk. A defining characteristic of a traditional main street is the mix and variety of storefront styles and types, in addition to the narrow widths and high level of transparency that lends to the vibrancy and animation of the street. New development should establish these characteristics according to the following guidelines:

- A strong base should be designed to reinforce the pedestrian nature of the uses on the street, and residential uses above should be clearly distinguished;

- Front façades must be highly articulated with a variation in massing, vertical breaks, colour, materials, brick detailing and window treatments;
- Larger commercial spaces should be broken up through vertical features and details (i.e. pilasters, columns, entry and main window surrounds, etc.) to give the appearance of smaller individual buildings;
- To ensure a strong and attractive visual presence at grade, adjacent to the public street and a high quality retail space at grade level, a minimum 4.0 metre floor to second floor height should be provided; and,
- An identifiable break or gap should be provided between the street-level uses and the upper floors of a building to emphasize the storefront, while adding visual interest and variety to the streetscape. This break or gap may consist of a change in material, change in fenestration, or the addition of a cornice line.

##### Community/Institutional

- The height and massing of community/institutional buildings should reflect their significance as feature elements of the community and enhance their identity; and,
- Massing elements such as major glazed openings, and landscape features should be utilized to emphasize the importance of community/institutional buildings.





### 3.2.3 BUILDING ELEMENTS

To encourage continuity in the streetscape and to ensure horizontal 'breaks' in the façade, buildings should be designed to reinforce the following key elements through the use of setbacks, extrusions, textures and materials. All buildings should be of a consistent architectural character to reinforce a cohesive image and reflect a sense of community within the East Fonthill Mixed Use Centre.

#### GENERAL GUIDELINES FOR ALL BUILDINGS

- In order to create a more pedestrian scaled and oriented streetscape, large scale buildings shall be broken up with architectural details, colours and materials to create a base at street level. Taller buildings shall be designed to express a base, middle and top through the use of materials and design elements;
- Architectural elements to promote a pedestrian scale at street level shall be incorporated through the use of patios for ground level units, common areas and primary entrances;
- Primary entrances should be prominent and clearly discernible along the building façades that face a public street. Entrances to buildings shall coordinate with the placement of pedestrian walkways and sidewalks;
- Buildings shall be well articulated to avoid long uninterrupted façades;
- Where elevations flank onto open space they shall be of similar quality to the front façade including consistent materials, window styles and detailing;
- Where upper storeys are proposed, windows should be vertically aligned from floor-to-floor and horizontally aligned with the neighbouring buildings; and,



- All buildings within the East Fonthill Mixed Use Centre shall:
  - Provide the greatest attention to design detail and material quality to the first 3-storeys of any mid-rise or high-rise building;
  - Relate to the architectural design and details of adjacent/existing built form;
  - Integrate steps and ramps into the architecture of the building (within the building envelope);
  - Create a consistent and articulated (vertically and horizontally) street wall condition that results in a pedestrian friendly – human scaled environment;
  - Differentiate units within the same building through wall plane variation, projections/recesses and, use of colour and materials, while maintaining a cohesive design;
  - Be designed to reduce the impact of lighting or noise on adjacent uses; and,
  - Be of a consistent architectural character to reinforce a cohesive image.

#### USE-SPECIFIC

In addition to those general guidelines, the following use-specific guidelines shall be considered:

##### Retail Storefronts and Mixed Use

- Provide special treatment to the top of stand-alone retail buildings by providing a distinct cap to the façade;
- Mixed use buildings should provide for pedestrian activity at the public street and provide greater access to retail uses for the residents of the community;
- The design of these buildings should clearly distinguish between retail, office and residential uses;

- Should the building exceed 2 to 3 storeys in height, a change of materials or setback/stepback should be incorporated to provide a sense of transition to the street;
- The building entrance to above grade residential units should be designed as a key component of the façade;
- On the upper levels of mixed use developments (2nd storey and above) encourage the use of balconies;
- Ground level floor-to-floor height should allow for conversion from residential to commercial uses over time;
- Utility meters shall be clustered or 'ganged' and enclosed through masonry, recessed in the design, and fully screened from view;
- Storefronts (the at-grade retail frontage) should have a high-level of transparency, with a minimum of 75 percent open glazing interior visibility to 1.8 metres to the interior of the building to maximize visual animation;
- Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade;
- Clear glass should be used for wall openings (e.g., windows and doors) along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts;
- On corner sites, storefronts should address both street frontages through entries and/or glazing; and ,
- Weather protection for pedestrians is encouraged through the use of awnings and canopies. Awnings:
  - Should be no more than 25 percent of the front façade;

- Should be made of high quality canvas, cloth, metal or glass material, and a contain a maximum of two colours; and,
- Canopies, signage and street furniture should be coordinated along a street block.

#### High and Medium Density Residential

- The elevation of the building shall include variation between units, while reinforcing common characteristics that visually unite the block;
- Utilities metres should be integrated into the design of the building so they are fully screened from view;
- Air conditioning units shall be discretely located at the rear of all units (i.e. garage area recess or on deck level and screened from view); and,
- Architectural elements to promote a pedestrian scale at street level shall be incorporated through the use of patios for ground level units, common areas and principle entrances.

#### Community/Institutional

- Community/Institutional buildings should be designed as special landmark buildings with high quality design, materials and finishes. The site should be well landscaped in recognition of their prominent locations and status as landmark buildings; and,
- The front door of all Community/Institutional buildings shall be connected with a walkway to the sidewalk on the public streets.

### 3.2.4 MATERIALS

New development should be mindful of ensuring excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of these Design Guidelines is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

#### GENERAL GUIDELINES FOR ALL BUILDINGS

- Building materials should be chosen for their functional and aesthetic quality, and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance;
- Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete;
- In general, the appearance of building materials should be true to their nature and should not mimic other materials. Materials shall be of a high quality and durable;
- Vinyl siding, plastic, plywood, concrete block, darkly tinted and mirrored glass, and metal siding utilizing exposed fasteners are discouraged;
- Clear glazing shall be incorporated on any façade that overlooks areas of public activity;
- On large expansive walls, active images, signage, green walls, and/or public art should be part of the design;
- Reflective glass in not acceptable at grade or upper level windows/curtain walls; and,
- Windows should be thermally sealed and double glazed.

#### USE-SPECIFIC

In addition to those general guidelines, the following use-specific guidelines shall be considered:

##### Retail Storefronts and Mixed Use

- All façades of retail buildings facing a street or parking area must be designed to the same standard of quality;
- Other façades of the building must contain materials that are in the same family of materials on the primary façade;
- Materials on the primary façade of retail pads should be consistent with that of other retail buildings in the block;
- Materials at the primary façade of retail buildings should be the highest quality including stone, brick and other masonry products;
- The transparent components of the retail façade should act to accentuate the other material; and,
- The materials on corner buildings must be consistent on all façades visible from the street.

##### Community/Institutional

- Community/Institutional buildings should be designed to reflect their importance through the use of unique materials and colour palettes that distinguish them from other types of development;
- Primary materials should be limited to glass, masonry and stone. Other high quality and durable materials such as iron, may be considered;
- All four elevations shall be consistent in materials, architectural details and fenestration, and shall be of a consistent design quality to address the high visibility of these buildings within the community; and,
- Large exposed walls without windows should develop relief through the use of fenestration elements and the change of materials and surface finishes.





### 3.2.5 SIGNAGE

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape, and should reflect the unique characteristic of their context. This includes compatibility with existing buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged. Commercial storefront signage shall be consistent with the following guidelines:

#### GENERAL GUIDELINES ALL BUILDINGS

- Storefront signage should add diversity and interest to the street, and not overwhelm either the storefront or the streetscape;
- Signage should aid pedestrians and drivers in navigating the area, especially at night;
- Signage should be integrated into the architecture of a building, and should not overwhelm the building and/or the storefront;
- Signage should work to reinforce the horizontal separation between the retail component and those uses of the upper level(s);
- To minimize visual clutter, signage should be integrated into the design of building façades wherever possible, through placement within architectural bays and friezes;
- Signs should be placed in a consistent location on all building façades. Generally, they should be located above storefront windows or canopies over the storefront. Signage will be allowed on the window or cornice, where it does not obscure these elements and are integral to the sign;
- Signs and lighting should be designed to complement the building and enhance the visual appeal of the street;

- Back-lit illuminated rectangular sign boxes are not permitted;
- Signage should not obscure windows, cornices or other architectural elements;
- Projecting/hanging signs should be permitted with permit to encroach over the streetline provided that they do not project more than 1.0 metre from the building, and they have a minimum 2.4 metres clearance between the bottom of the sign and grade;
- Signage should be proportionate with the façade and massing characteristics of the building;
- Highly animated and illuminated digital signage should not be permitted;
- The ratio of sign band to building mass should be restricted such that the signage does not dominate the façade;
- Ideally, signage should be comprised of back lit cut out letters;
- Large freestanding signs (such as pylons), roof signs, and large-scale advertising (such as billboards) are discouraged;
- Moveable signs like sandwich boards should have two sign faces, and be located in front of the associated business on private property where possible, but are permitted to encroach onto public property with permit provided that they:
  - have an appropriate permit;
  - provide a minimum 1.5 metres of unobstructed sidewalk space; and,
  - do not exceed a maximum size of 0.6 metres wide by 1.2 metres high, or 0.9 metres in depth; and,
- Signage that identifies uses within the proposed East Fonthill Mixed Use Centre will be limited to specific locations. The following guidelines will apply:
  - Signage that identifies multiple users on large sites will be located on pylon signs at the identified locations;
  - Signage pylons will be of a consistent design throughout the East Fonthill downtown;
  - All other signage will be restricted to the building façades; and,
  - Multiple businesses are encouraged to group together on pylon signs.



### 3.2.6 ACCESS, PARKING & SERVICE AREAS

Access refers to the location and scale of the pedestrian and vehicular entry to a lot and/or building. The primary objective with respect to parking and servicing is to minimize their visibility and potential conflicts with pedestrians. New development shall be consistent with these guidelines:

#### GUIDELINES FOR ALL BUILDINGS

- Primary pedestrian building entrances should have access from the street. Secondary building entrances may be oriented toward the rear or side of the building for access to and from parking lots;
- Clear pedestrian routes should be provided from parking areas to main entrances;
- Pedestrian walkways between buildings should provide for a clear view, from one end to the other, to ensure pedestrian safety;
- On-street parking should be provided where possible;
- Surface parking will not be allowed in front of the building facade at the street;
- Parking for development should be provided below-grade or to the rear and accessed by a rear lane, where possible;
- Surface parking and service areas should be located to the rear of a development, screened by buildings or landscaping, and generally not visible from the street;
- Where surface parking or service areas are exposed, they should be concealed with landscaping and/or other mitigating design measures;
- A sufficient planted buffer should separate parking lots from the pedestrian sidewalk;
- Loading and service areas should be screened from adjacent properties through the use of fences and enhanced landscaping;
- Direct access to parking or service areas from primary streets is discouraged;
- Large parking areas should be broken down into smaller parking courts of 100-150 cars spaces (maximum). Parking courts should be defined with landscaped edges and pedestrian walkways;
- Planting edges must be of sufficient width to ensure successful plant growth;
- Parking lighting should be designed in accordance with the Town's lighting standards;
- No parking shall be permitted at corner conditions;
- Surface parking shall be designed as courtyards, screened from public view and include raised landscaped medians at every double-loaded row of 10 (total 20 parking spaces);
- Ideally, service/loading areas should be integrated within the massing of the building and designed as an integral part of the architecture; and,
- Siting of future buildings should be undertaken to allow structured parking to be developed over time, with the exception of neighbourhood retail.

#### USE-SPECIFIC

In addition to those general guidelines, the following use-specific guidelines shall be considered:

##### Community/Institutional

- Parking should be located strategically so as not to interrupt the flow of traffic, (i.e. pedestrian, cycling and vehicular);
- Drop-off and parking areas should not be located between the building and the street; however, when site constraints necessitate front yard parking, a landscaped or architectural element should be provided to define the street edge;
- All vehicular parking and private vehicle drop-offs are to be located at the least visually prominent side and/or the rear of the building; and,
- Bus pick up and drop off areas that are separate from other traffic should be provide on-lot at the side of the building, but may be located in the front of the building subject to building and site plan consideration.





### 3.3 PUBLIC REALM GUIDELINES

The public realm is comprised of public roads and their associated spaces and amenities (traffic circles and woonerfs, and streetscaping), open spaces such as parks and urban squares, the connected trail network and stormwater management facilities. This section provides general guidance for the design of these major components of the public realm.

The public realm guidelines are intended to help guide and shape the design and development of the public realm within the proposed East Fonthill Mixed Use Centre. They establish the guiding principles that will inform detailed design. The proper design of the public realm will enhance the quality and experience of the community. All development within the proposed East Fonthill Mixed Use Centre shall be consistent with these Design Guidelines.

DEMONSTRATION PLAN FOR PUBLIC REALM



### 3.3.1 ACTIVE TRANSPORTATION

Development in the proposed East Fonthill Mixed Use Centre will accommodate a street network made up of a Ceremonial Route, a Main Street, a Local Greenway and a Collector Street. The network also includes, and is connected to Regional Road 20 (Hwy 20) and Rice Road (RR54). The proposed street network is linked to Fonthill's existing road network and the trail system, all defined conceptually on the Development Concept. In order to ensure maximum efficiency, connectivity, and mobility within the community, pedestrian, cyclist, and vehicular movement should function as an integrated network.

#### GENERAL STREETScape ELEMENTS

Streetscape elements are considered elements of detailed design within the street Right-of-Way that enhance the character and functionality of space. Elements include street furniture (waste receptacles, signage, newspaper boxes, community mail boxes, information kiosks, bike racks, transit shelters and benches), street trees, street lighting, special pavement and planting. The following guidelines generally apply to all streetscaping projects in the East Fonthill Mixed Use Centre:

- Street trees will be located in the boulevard on both sides of the street;
- Tree species should be planted to form a connected urban canopy at maturity, or 8.0 to 10.0 metres on-center;
- Street trees should generally be planted opposite each other on each side of the street,
- To minimize clutter and visual disruption of the road's character, poles, lights, signs and other services should be located along the street tree planting line,
- Height and style of street lighting should be consistent

with the hierarchy of the road, and lighting design should accommodate both pedestrian and vehicular movement and visibility requirements within the right-of-way (ROW);

- Mailboxes, trash cans, and recycling bins should be clustered together in single locations and placed adjacent to open spaces and/or commercial/community facilities. Molok type garbage receptacles are encouraged; and,
- Utility boxes, including transformers, should be placed in low visibility spaces (flankage locations, open space, laneways and private property or within the boulevard) where they can be screened with planting and fencing.

#### STREETSCAPING FOR TRANSIT

The following guidelines are intended to enhance the attractiveness and convenience of public transit when it is introduced to the proposed East Fonthill Mixed Use Centre:

- Transit stops should be located as close to intersections as possible, and their location coordinated with sidewalks and other trail connections and building entrances;
- Transit shelters should be designed with transparent sides for maximum visibility to and from the interior, so that transit users can see approaching buses and to maximize pedestrian safety;
- Shelters should be located on the boulevard adjacent to the pavement to maximize passenger convenience;
- Curbside transit stop loading areas should be a clear, hard surface area 1.5 to 2.0 metres wide in front of a shelter to permit safe exit by passengers, including wheelchair users. In all cases, shelters should be set back 0.5 metres from curbs and sidewalks to protect them from damage by snow ploughs;
- Surface texture changes should be provided at transit stops to assist the visually challenged in locating the stop and/or shelter location;

- Bollards with shields should be incorporated as pedestrian protection areas at all major transit stops;
- Where four-sided transit shelters are not possible, overhead open-air canopies should be provided to protect transit users from sun, rain and snow; and,
- Transit stops shall be designed to offer amenities such as seating areas and weather protection. Benches and other roadside furniture such as waste baskets, bike racks, telephones, notice boards, newspaper boxes and refuse containers should be concentrated at bus stops along the main street collectors to maximize their utility and create active public space.

#### SHARED STREETS

Shared Streets, often known as Shared Spaces, allow pedestrians and vehicles to share the same areas without the traditional hard distinctions such as curbs and sidewalks. These streets are not typically paved with asphalt, rather they feature a variation in materials including decorative pavers to define the space. All modes of transportation are given equal priority on this type of street. The space is used by pedestrians and acts as a host for outdoor activities, becomes a destination and is more vibrant than a traditional street. A Woonerf (dutch term for 'living street') is a form of a Shared Street, it gives pedestrians priority over all other users. Speed limits are intended to be very low, almost equivalent of walking speed to discourage vehicles from using the street.

#### TRAILS

A trail network is identified conceptually on the Development Concept Plan. It will link pedestrians and cyclists throughout the proposed East Fonthill Mixed Use Centre, the Community of Fonthill, and throughout the Town of Pelham along natural features, stormwater ponds, and open space/parks. The network will also include urban trails within public roads. The design of trails will change in character as it travels through the various areas of the community.

The following are objectives to be achieved for the trail network:

- The preferred locations for the trails network are within or adjacent to open space features and natural areas and opposite abutting Residential yards, where possible;
- The trail network will accommodate cyclists and pedestrians, and will provide links to on-street bicycle lanes and sidewalks along roads;
- Trails should have a minimum width of 3.0 metres, where possible;
- Trail design will be based on each site's sensitivity in order to minimize environmental impacts;
- Trails should be clearly signed regarding permitted use and speed. Wayfinding signage should be provided throughout the trail network (including the on and off-street components);
- Trails should be designed to reflect safe passage and restrict access to neighbourhood properties;
- Benches, Molok type waste and recycling receptacles, lighting, bicycle racks and natural or built shade structures should be provided at trail heads and at regular intervals along the trails;
- Trails located in proximity to sensitive natural features or adjacent to stormwater management facilities should incorporate interpretive signage at various locations to promote stewardship initiatives that will protect and enhance the features and functions of the natural environment;
- Where trails intersect with motorized vehicle infrastructure or roads, clear signage and safety features will be provided for the safety of both the trail user and motorized vehicle user; and,
- The Steve Bauer Trail is to be preserved, connected and incorporated within the new trail network.

Overall, the trail system for the proposed East Fonthill Mixed Use Centre is designed to connect the open space system

and street network with existing and proposed trail networks connecting downtown Fonhill and the neighbourhoods of the East Fonhill Secondary Plan to the south. The trails network with the proposed Mixed Use Centre will consist of:

- Multi-Use trails within natural areas, open space and along Rice Road (RR54);
- Urban trails, a combined trail and sidewalk within the public road Right-of-Way that may accommodate pedestrians and cyclists; and,
- Pathways in parks and other open space blocks.

The main pedestrian trails will incorporate an appropriate level of lighting to ensure that they meet CPTED principles. Seating areas will be incorporated along the main path ways in the parks and open spaces.

### DEMONSTRATION PLAN FOR ACTIVE TRANSPORTATION



## STREET TYPOLOGIES

This section of the Site Master Plan identifies the hierarchy of streets that are intended to encourage various modes of transportation. Detailed streetscape design for each of the typologies is provided in (Part 2) of this document. General Principles for streetscape design include:

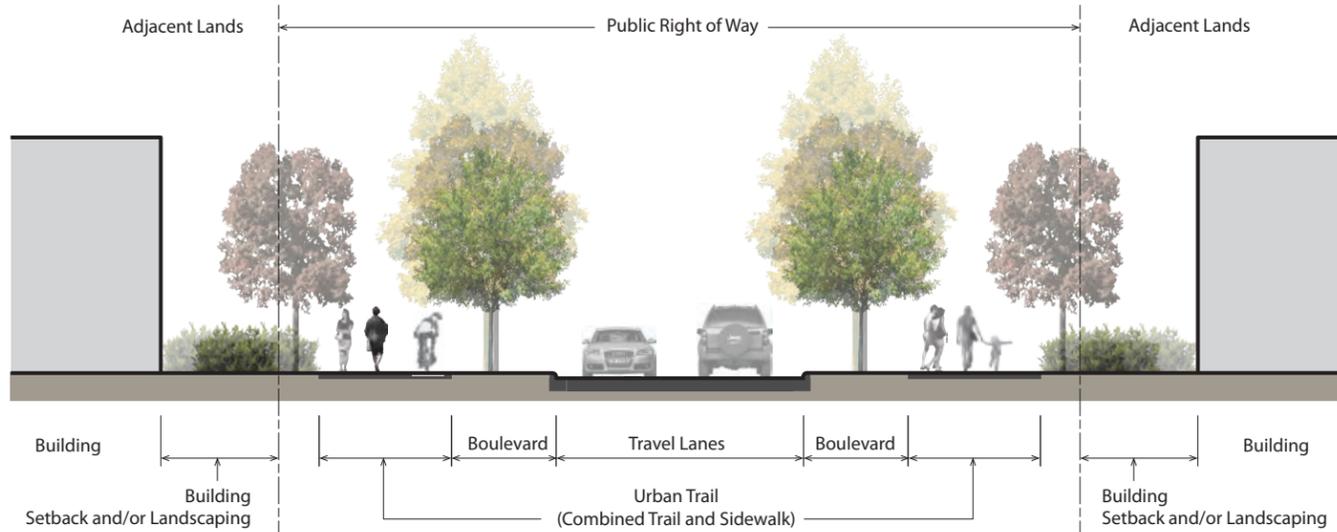
- Provide clear, safe and efficient access to community facilities such as commercial areas, and open space features.
- Ensure pedestrian access throughout the community by providing sidewalks and trails throughout the community.
- Promote tree-lined and well-shaded streets to create sense of comfort and promote walking and cycling.

The components of the street include the elements within the Right-of-Way and the elements within the adjacent lands. Streetscape design considers both areas comprehensively and with particular focus on the transition between the two.

The types of streets that form the Mixed Use Centre, as shown on the Demonstration Plan for Active Transportation include:

- Ceremonial Route;
- Main Street - Wellspring Way;
- Local Greenway;
- Collector Street C;
- Regional Road 20 (Hwy 20); and,
- Rice Road (RR54).

### ACTIVE TRANSPORTATION TYPICAL STREET CROSS SECTION



Examples of a median at community entries



Examples of Main Streets

## CEREMONIAL ROUTE

This street is designed as the key link between the historic downtown of Fonthill and East Fonthill. It is envisioned to be a tree lined promenade with a right of way of either 23.0 metres or 28.0 metres (with a landscaped centre median) that is pedestrian and cycling friendly and designed to reduce the speed of vehicular traffic. Along the Nature Park street frontage, the Right-of-Way will be 28.0 metres to incorporate a 5.0 metre wide treed / planted central median. Urban trails will be incorporated in both the 23.0 and 28.0 metre wide right-of-way (ROW).

The Ceremonial Route also incorporates roundabouts and/or woonerfs or raised intersections that are pedestrian priority zones, located at each end of the Nature Park. Enhanced landscaping, including special paving for the road surface will encourage traffic calming and signal to motorists that they are entering a pedestrian priority area.

## MAIN STREET - WELLSPRING WAY

The East Fonthill Mixed Use Centre is focused on a Main Street. This street will be animated by the commercial spaces that line it, including shops and restaurants. It will incorporate parking on both sides of the street to support the commercial function. Street trees with seasonal planters will line both sides of the street. The street cross section will also provide designated space for bi-directional bicycle lanes. Street furnishing such as benches, Molok type trash receptacles and newspaper boxes will be located at intersections. Bicycle rings will be located along the curb edge of the side walk at regular intervals.



Examples of Local Greenway streets

### LOCAL GREENWAY

The Local Greenway is a special street that is intended to become an ecological corridor, providing a linkage from RR20 (Hwy 20) to the natural heritage system at the south portion of the Secondary Plan. The cross section provides for one lane of travel in each direction with parking on one side of the street as well as designated bicycle lanes.



Examples of Main Collector Streets

### COLLECTOR STREET C

Collector Street C is the main east-west service street linking the proposed Mixed Use Centre. From Rice Road (RR54), it leads into the Main Urban Square and provides access to a number of development sites as well as providing a main urban trail linkage between Rice Road (RR54) and the Greenway.

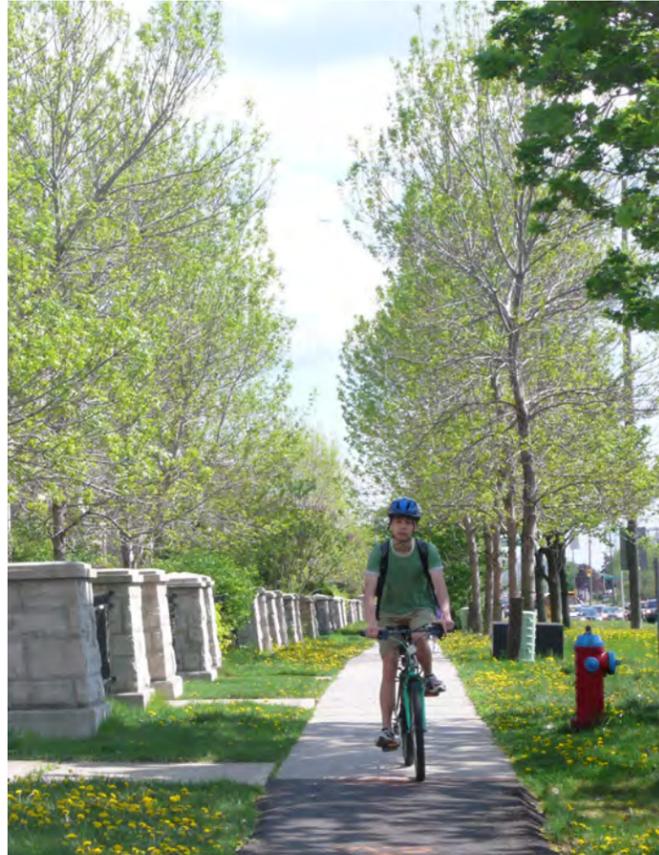
At the intersection of Street C with Main Street, coordinated and consistent streetscape design will be integral to the design of the square, not only through the streets and public space, but also to the building face of built form in the adjacent four corners, to create a continuous pedestrian plaza and share use space, as an extension of the urban square.



Example of a Regional Road

### REGIONAL ROAD 20 (HWY 20)

Regional Road 20 (Hwy 20) is envisioned to be a tree lined, pedestrian-scaled, mixed-use street. It defines the commercial street frontage, leading to the historic Downtown. It includes an important Gateway to the village of Fonthill, and will announce an arrival to Pelham. It is a Regional Road that has multiple functions. The goal of the Town is to ensure a unified and distinctive character, with a connected sidewalk and trail system and a beautiful and comfortable streetscape experience.



Example of a continuous trail parallel to the road

### RICE ROAD (RR54)

Rice Road (RR54) is an important Regional Road that connects RR20 (Hwy 20) south to Lake Erie via Highway 58 and Main Street, Port Colbourne.

In this location, Rice Road (RR54) represents the eastern limit of urban development in the Town, with primarily rural residences located on the east side of the road that are unlikely to be redeveloped in the near future.

The west side of Rice Road (RR54) provides a great opportunity create a continuous trail that provides linkages to the Steve Bauer Trails Network within this part of the Town, potentially connecting to Circle Loop 7 by way of Port Robinson Road.

### 3.3.2 PUBLIC OPEN SPACE

Public open spaces comprise critical structural elements of the proposed East Fonthill Mixed Use Centre. The system is focused on a centrally located Main Urban Square, and a Nature Park associated with the re-aligned existing stream. It is expected that other, smaller Neighbourhood Squares will be incorporated into the Mixed Use Centre as it evolves over time. It is the intent of the Town that the system of public open spaces become focal points for community activity and that they are wholly integrated and connected to the street network, trails network and system of stormwater management facilities. As a system, all of the open space areas and parks will work together to enhance the natural environment and ecological function of the community and promote sustainability in Fonthill.

#### PUBLIC OPEN SPACE





## NATURE PARK

The largest park in the proposed East Fonthill Mixed Use Centre, the Nature Park, is the natural centrepiece of the neighbourhood. Residents of the townhouses that border the site will use the open space as an escape where they can be in touch with the natural world. Winding pathways provide a place to walk the dog or go for a run or stroll. Trees and shrubs are grouped informally within a field of long grasses and flowers – the planting reflects the native vegetation of the Fonthill area. A large stormwater pond stores and treats the runoff from surrounding development before releasing the treated water to the adjacent stream. The connection to the nearby stream creates a corridor which allows for aquatic and terrestrial wildlife to move to and from the park into the larger watershed. Lighting should be designed having regard to the Town’s CPTED and lighting standards.

Existing natural heritage features and environmental areas are to be protected and enhanced. The following design guidelines are intended to preserve, protect and integrate the natural heritage features within the Nature Park:

- Development shall consider areas for re-planting the natural heritage edge is necessary to improve the visual appeal and ecological integrity of the edge (in consultation with the Town of Pelham);

- Development impacts shall be minimized by protecting the natural heritage edge from sedimentation, soil erosion, or damage to tree trunks and the canopy;
- A planting philosophy shall be adopted along the natural heritage edge that emphasizes drought tolerant and native species;
- Trails shall be strategically located in defined buffer zones where possible, to minimize soil compaction or disturbance to existing mature vegetation and significant natural features; and,
- Where appropriate, views of natural features should be enhanced by locating developed open space/ parkettes at the terminus of local roads and homes, which should face onto the natural features.



## MAIN URBAN SQUARE

The Main Urban Square is intended to be the community focus for East Fonthill and a counterpoint to Peace Park in the downtown. The Square extends from building face to building face across the intersection and includes all four corners. The uses along the edges of the square are intended to be primarily commercial/retail and institutional uses.

The Main Urban Square will function on a daily basis as an urban park and meeting place; it will also serve the larger community for special events and programmed activities. Design will encourage year round enjoyment of the space, including hard and soft landscape elements that will create seating, shade, gathering spaces and quiet places. It will also be designed for program flexibility, and with design integration of Collector Street C and Wellspring Way woonerf, potential expansion into the streets.



## NEIGHBOURHOOD SQUARES

Neighbourhood squares are intended to be used by the people living and working in close proximity. They are intended for passive use and include areas for sitting, meeting and neighbourhood related activities. They will be planted with trees and incorporate gardens, lawns and walkways. There will be permanent seating as well as pedestrian lighting (where practical), bicycle racks and Molok style trash receptacles. There should be clear views in and through the neighbourhood squares as well as seasonal interest and attractions. Trellises, low walls and other landscape features will be encouraged. Lighting should be designed having regard for the Town’s CPTED principles and lighting standards. The following design guidelines apply to Neighbourhood Squares:

- A Neighbourhood Square is a small component of the public open space system, that can be soft surfaced and green or hard surfaced;
- Where possible, Neighbourhood Squares should provide a link to other components of the public open space system, and act as destination points and rest areas for the trail system;
- Pathways within Neighbourhood Squares shall connect to pedestrian sidewalks on streets and the trail system;



- Neighbourhood Squares should be located to develop a highly visible and central gathering/socialization area for the surrounding neighbourhood, with visible road frontage, and clearly defined entries;
- View corridors terminating at a Neighbourhood Squares should be highlighted through landscape treatment and/or built form elements;
- A distinctive appearance should generally be achieved for Neighbourhood Squares through plant and construction materials;
- Neighbourhood Squares should include seating, lighting (where practical), shaded areas under tree canopies or open air structures, group mailboxes, hard surface areas, a memorial space feature and distinctive tree, shrub and ground cover planting material;
- Views to the Neighbourhood Squares should be maximized while ensuring visual privacy for adjoining residents; and,
- Street trees should be planted along the edge of the Neighbourhood Squares, while enhancing views into the park.

## GATEWAYS

Gateways are identified on the Demonstration Plan for Active Transportation. It is the intent of the Town to develop these identified Gateways to establish a sense of arrival and entry into the community. Coordinated landscaping, built form, high-quality development, and streetscape design will be required to achieve this goal.

The design and development of the identified gateways shall be guided by the following:

- Distinct, landmark buildings are encouraged to be located at gateway corners;
- Enhanced streetscape design and landscaping shall be provided in these locations;
- Enhanced landscaping may include a combination of signage, planting, lighting, architectural walls/columns, decorative paving, etc;
- The Ceremonial Route and Main Street into the community should include a landscaped centre median and other design features to signify these gateway locations; and,
- Gateway intersections should have distinctive surface treatment for pedestrian crossings, including wider sidewalks.

## STORMWATER MANAGEMENT FACILITIES

The stormwater ponds and channel facilities within the proposed East Fonhill Mixed Use Centre are designed to help with the infiltration and treatment of run-off while promoting sustainability through the provision of habitat and enhancement of ecosystem functions. Stormwater ponds will also provide passive open space that will address not only the immediate neighbourhood, but the entire Fonhill community.

- Stormwater management facilities shall be designed as integrated landscape features that provide a neighbourhood amenity, while achieving functional objectives related to stormwater flow moderation and water quality;
- Ponds are envisioned to blend with the natural landscape, therefore the slope grading of stormwater facilities should ensure natural and variable side slopes and sinuous contours. Inlet and outlet structures will be concealed using a combination of planting, grading and natural stone;
- The banks of stormwater facilities should be stabilized with flood tolerant waters edge plants and native species, including herbaceous and woody vegetation. The perimeter pool shall be planted with emergent, strand and sub-emergent species to improve the aesthetics and enhance the performance of the facility;

- Where there is a need to discourage public access to areas around the perimeter of the ponds, living fences and barrier planting will be used in place of fencing (where possible). Barrier planting will be comprised of multiple rows of predominantly thorn bearing shrub species. Barrier planting will be installed along the crest of steep slopes, adjacent deep-water areas and around inlet and outlet structures;
- At no time will chain link fencing be permitted around a stormwater management pond;
- The stormwater ponds shall be accessible by pedestrian and cycling trails that will be part of, or provide connections to, the broader trails network;
- To ensure the ponds are accessible to pedestrians and cyclists, they shall not be fenced, when possible; and,
- Stormwater channels may be designed with meandering and naturalized slopes in designated parks.



## GATEWAY POND

The Gateway Pond is the stormwater management facility located at RR20 (Hwy 20) and Rice Road (RR54). In this location it will form a gateway into Pelham. Also, in this location, it serves as a public open space for the neighbouring residents of the proposed seniors residence and community at large. The pond will be naturalized, including a diverse and native palette of upland and lowland species that will enhance the function of the facility while providing potential habitat for birds and other fauna. The transition/interface with adjacent development should include coordinated landscaping and pedestrian connections.



## SOUTH POND

The South Pond is the stormwater management facility located at the intersection of the Ceremonial Route and the Main Street (Wellspring Way). It is intended that this facility be integrated with the abutting Nature Park. The pond will be naturalized, including a diverse and native palette of upland and lowland species that will enhance the function of the facility while providing potential habitat for birds. The transition/interface with adjacent development should include coordinated landscaping and pedestrian connections.



## 4.0 DETAILED DESIGN PUBLIC REALM

Within the proposed East Fonthill Mixed Use Centre, the public realm is a system of streets and open space that collectively work together to enhance the function, quality and experience of the community. As such, it is important that implementation happens in a coordinated manner and the approach to design is consistent and well thought out to reflect the vision, character and identity of East Fonthill.

The public realm consists of streets, parks, urban squares, open spaces and the trails and pedestrian linkages that enable these to function as one coherent and highly interconnected system. This section of the document illustrates conceptual designs for these areas of the public realm and describes the elements that will be used to make them successful places/spaces.

# DEMONSTRATION PLAN FOR THE PUBLIC REALM



## 4.1 STREETSAPES

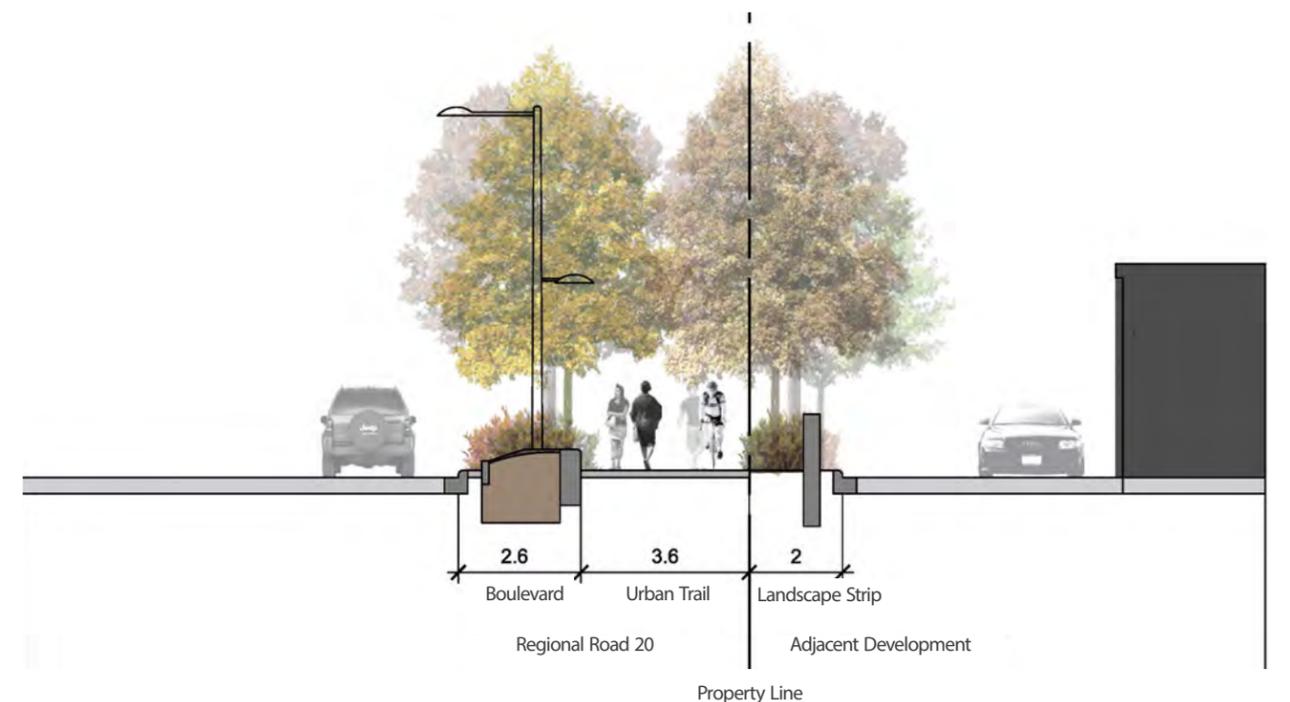
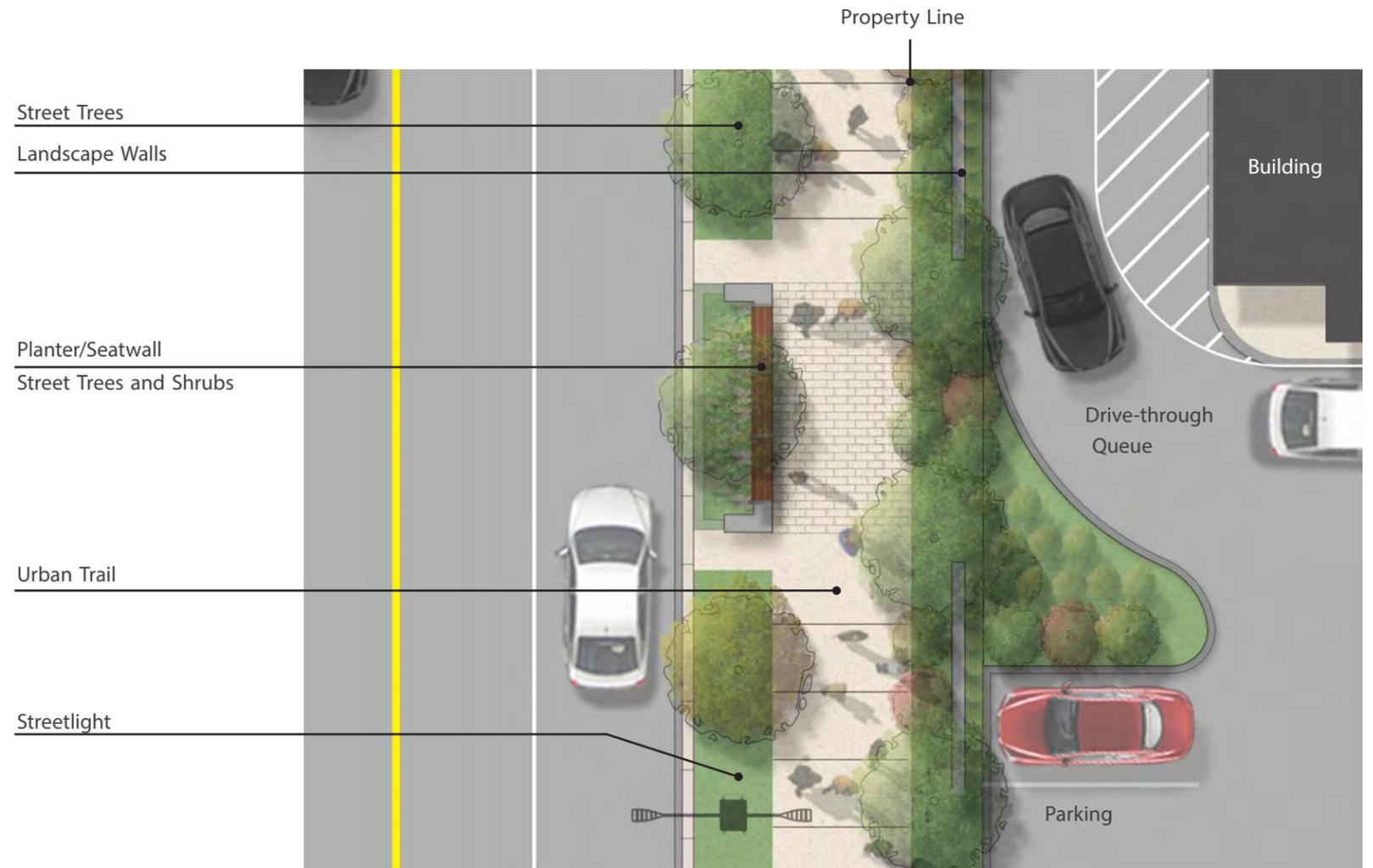
### 4.1.1 HIGHWAY 20 (RR20): A MAJESTIC ALLÉE

Regional Road 20 (Hwy 20) is the most important entry route into the Town of Pelham from the Niagara Falls area, neighbouring municipalities of St. Catharines and Thorold. Regional Road 20 is envisioned to be a tree lined, pedestrian-scaled, mixed-use street. As the street trees mature, the street is intended to give a sense of a Majestic Allée for both pedestrians and drivers with tall mature trees and their overarching branches reaching across the road. Regional Road 20 (Hwy 20) combines with Pelham Street to define the primary commercial promenade of the Village of Fonthill.

#### DESIGN ELEMENTS WITHIN THE RIGHT-OF-WAY

- A 3.6 metre wide urban trail located along the lot line on both sides of the street;
- Large, deciduous canopy trees planted within the boulevard (or planter), in groups of three, alternating species of oak trees - Red Oak, Shingle Oak and Chinquapin Oak. The minimum caliper of trees at planting will be 80 millimetres in diameter;
- Planter seat walls located within the boulevard, adjacent to the curb and planted with trees, shrubs and perennials;
- Pedestrian-scaled lighting located along the curb; and,
- Walkway connections to adjacent site/lots.

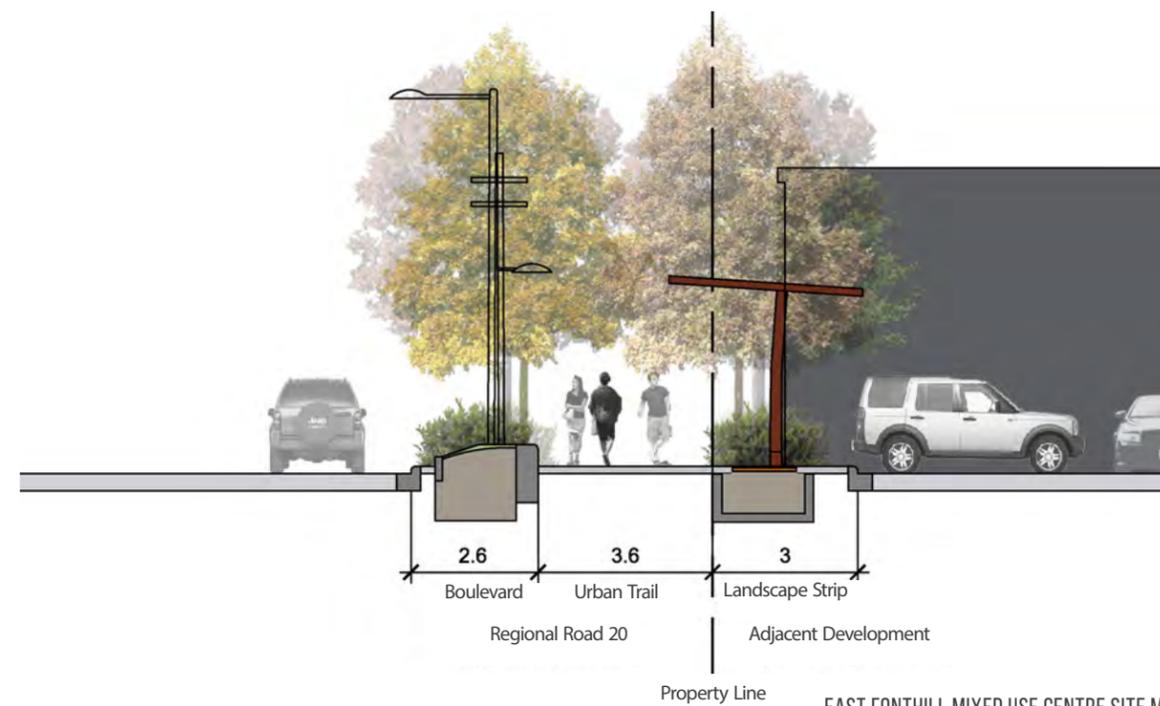
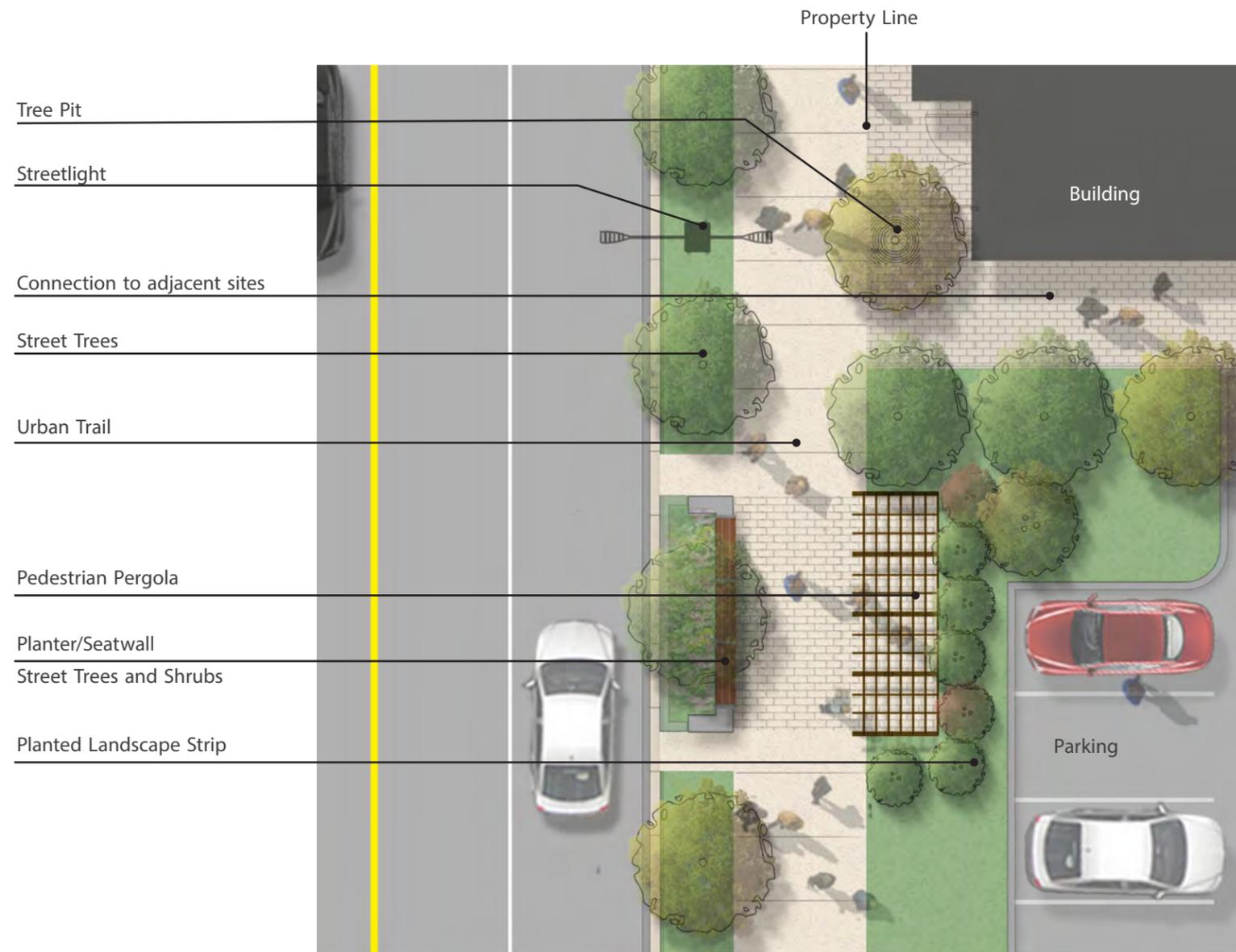
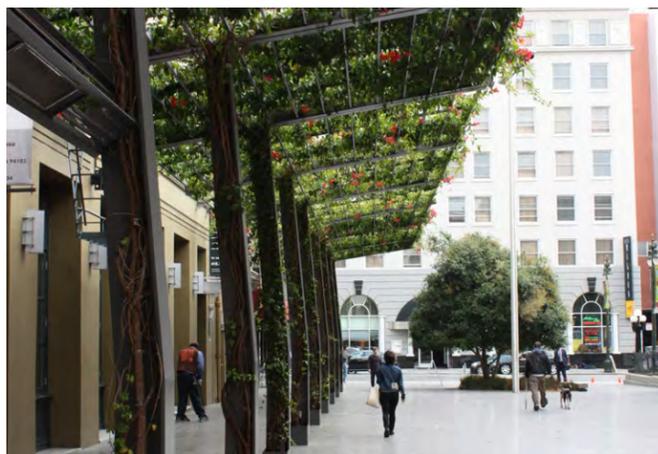
Landscape Walls



## DESIGN ELEMENTS WITHIN THE ADJACENT DEVELOPMENT WHERE PARKING AND QUEUE LANES OCCUR

A minimum 3.0 metre wide landscaped strip to consist of:

- Deciduous canopy trees, shrubs and perennials;
- Landscape walls; and,
- Pedestrian pergolas.

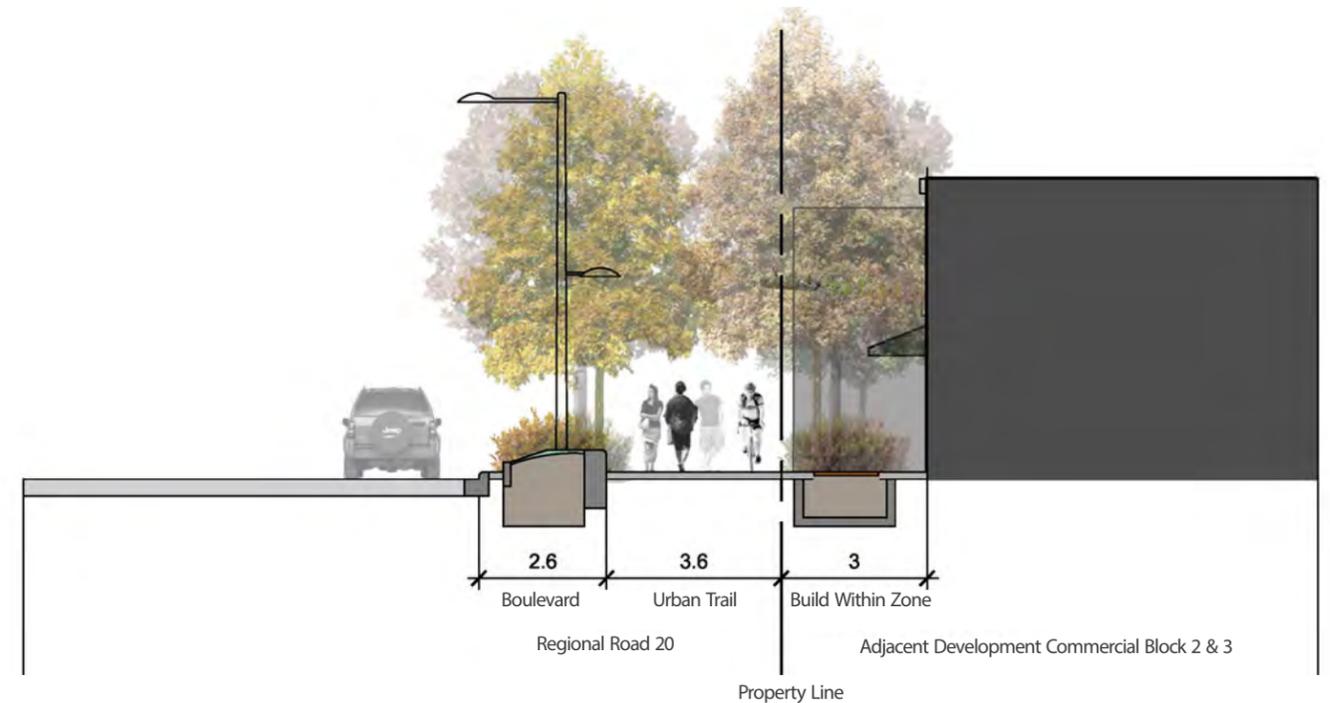
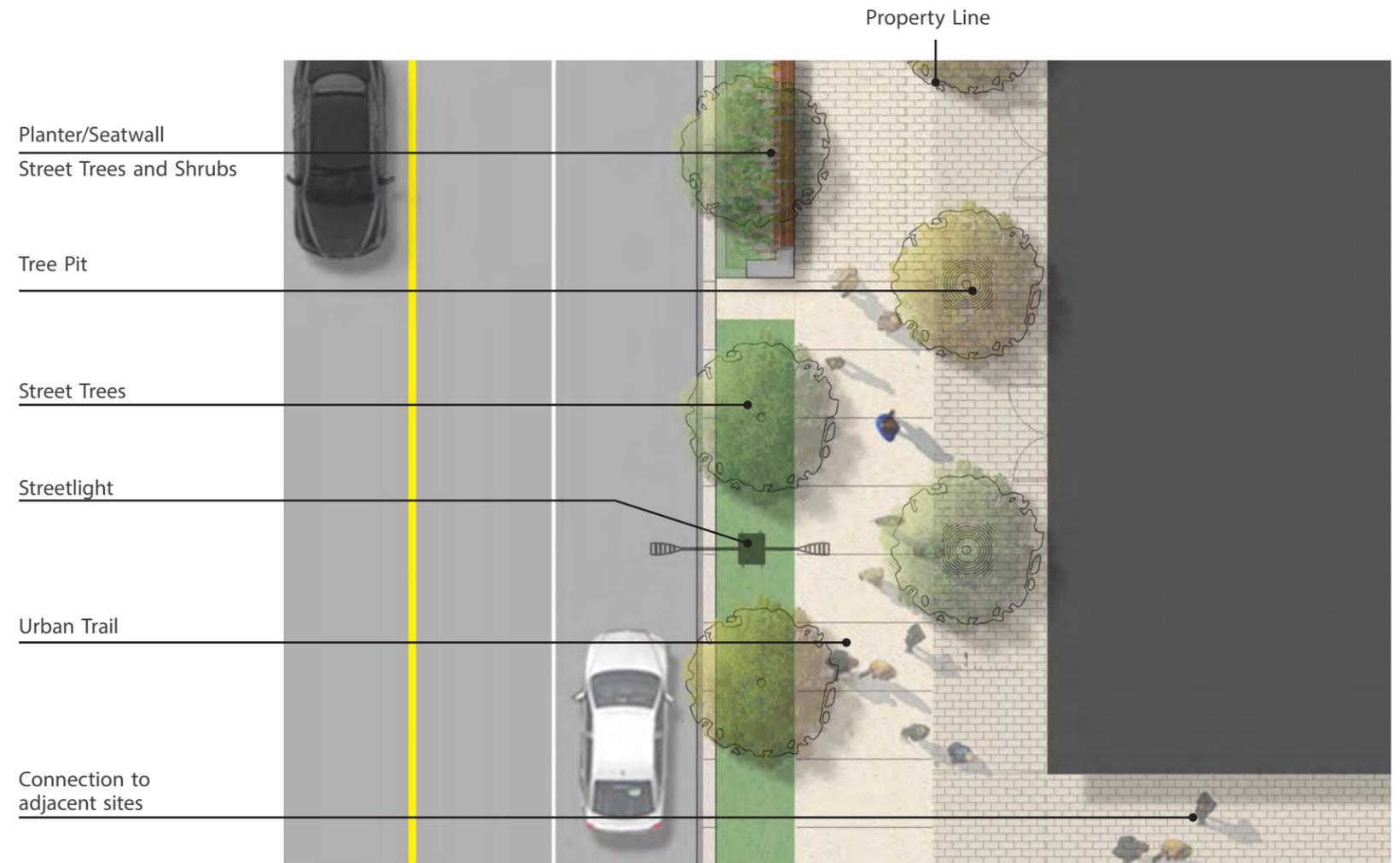


## DESIGN ELEMENTS WITHIN THE ADJACENT DEVELOPMENT WHERE BUILDINGS ARE LOCATED

- Continuous pavement to the face of the building;
- Trees in pits, where building entrance are located; and,
- Trees and shrubs in planters where blank walls occur.



Urban Trail Paving



## 4.1.2 RICE ROAD (RR54): THE RURAL TRANSITION

Rice Road (RR54) is an important Regional Road that connects RR20 (Hwy 20) south to Lake Erie via Highway 58 and Main Street, Port Colbourne, through Welland.

In this location, Rice Road (RR54) represents the eastern limit of urban development in the Town, with primarily rural residences located on the west side of the road that are unlikely to be redeveloped in the near future.

The west side of Rice Road (RR54) provides a great opportunity create a continuous trail that provides linkages to the Steve Bauer Trails Network within this part of the Town, potentially connecting to Circle Loop 7 by way of Port Robinson Road.

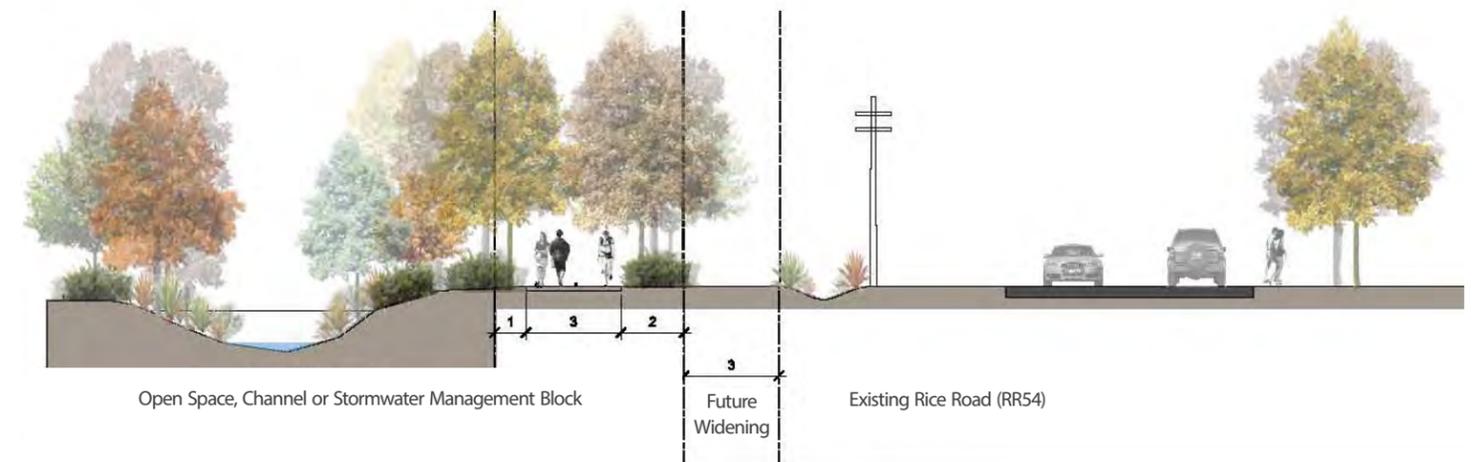
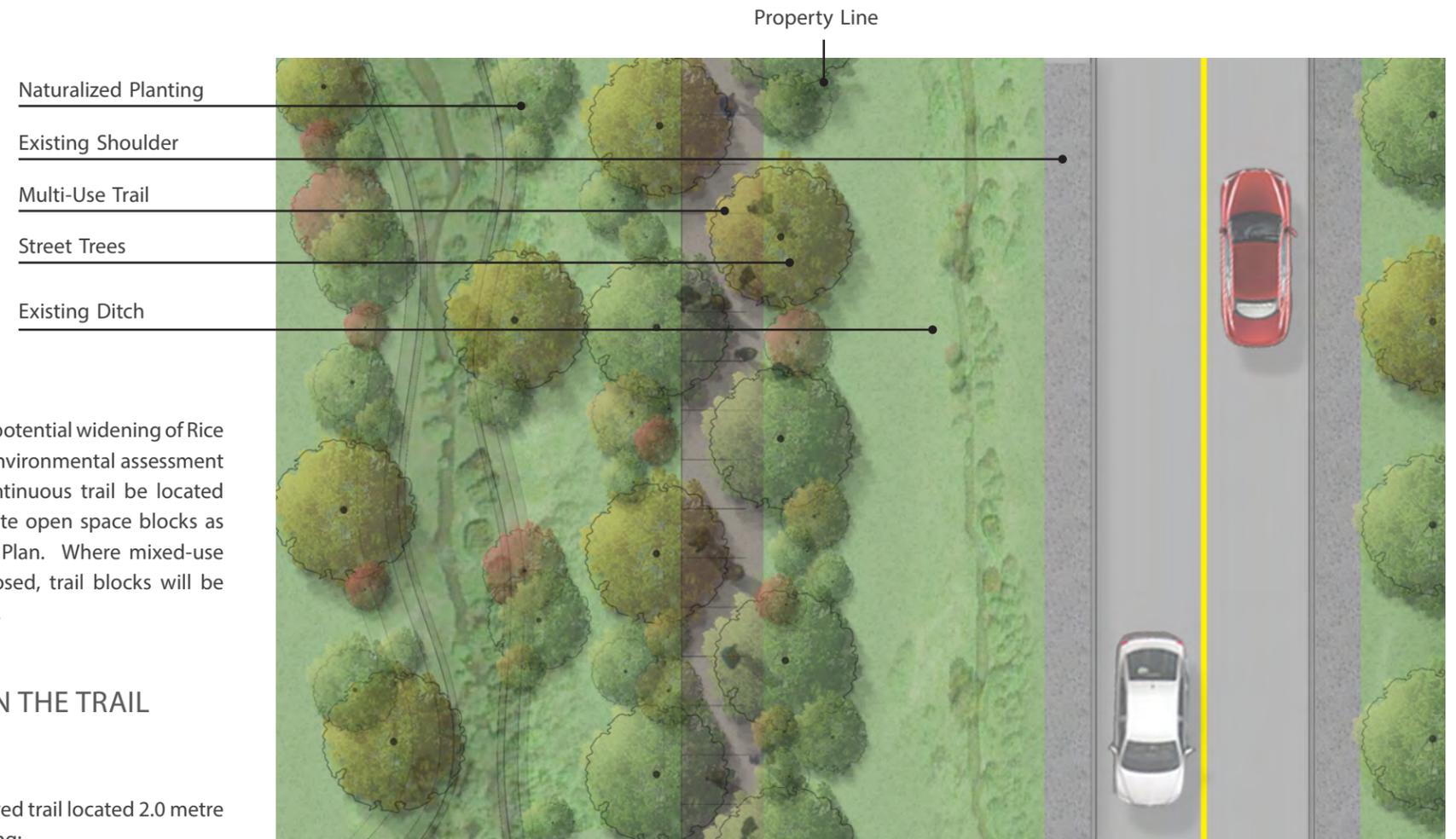


Multi Use Trails

Taking into consideration, the future potential widening of Rice Road (RR54), which is subject to an environmental assessment process, it is proposed that the continuous trail be located beyond the widening, within separate open space blocks as part of the East Fonthill Secondary Plan. Where mixed-use or residential development is proposed, trail blocks will be created at the time of development.

### DESIGN ELEMENTS WITHIN THE TRAIL BLOCK

- A 3.0 metre wide asphalt paved trail located 2.0 metre from the future road widening;
- Large, deciduous canopy trees planted on either side of the trail - Trembling Aspen, Basswood; and,
- Groupings of native shrubs and perennials between the trail and the future road widening.



## ELEMENTS WITHIN ADJACENT MIXED-USE BLOCKS

- Pedestrian connections to mixed-use sites and building entrances;
- Naturalized plantings within building setbacks; and,
- A minimum 3.0 metres wide landscaped strip where parking is located.



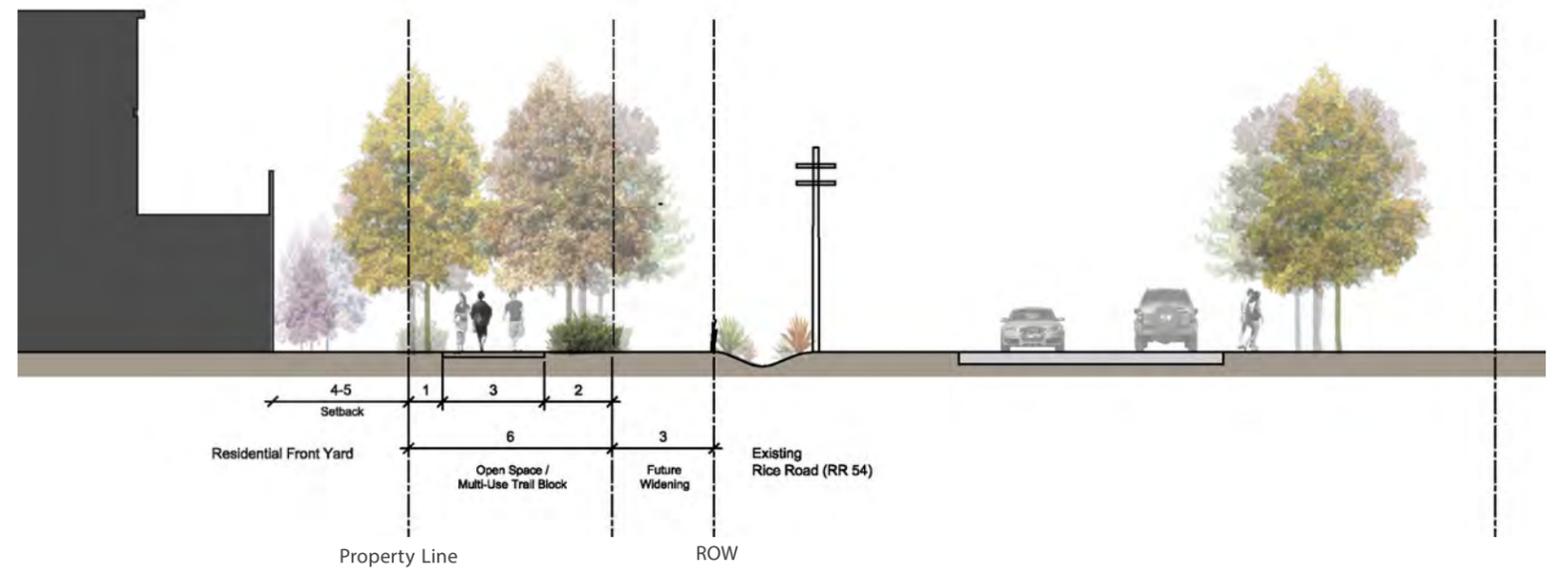
Planting adjacent to development



Naturalized Planting



Plantings

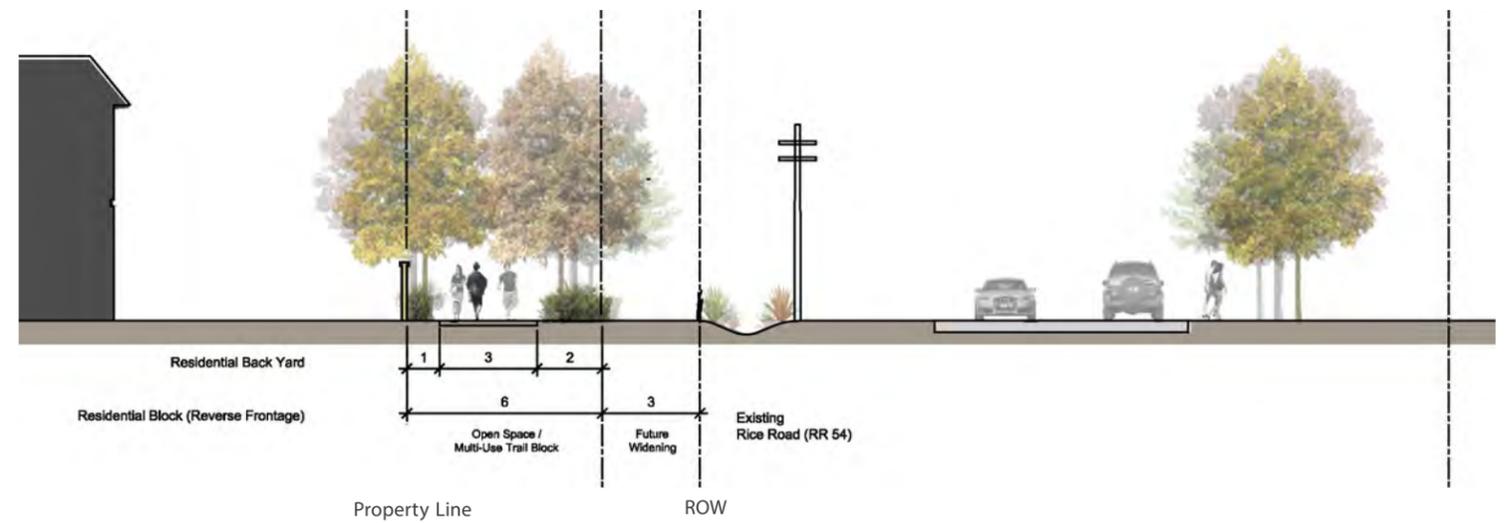


## DESIGN ELEMENTS ADJACENT TO RESIDENTIAL LOTS

- A continuous 1.6 metre high wood fence located along the rear lot lines abutting the trail block
- Naturalized planting between the fence and the paved trail to consist of deciduous trees and shrubs.



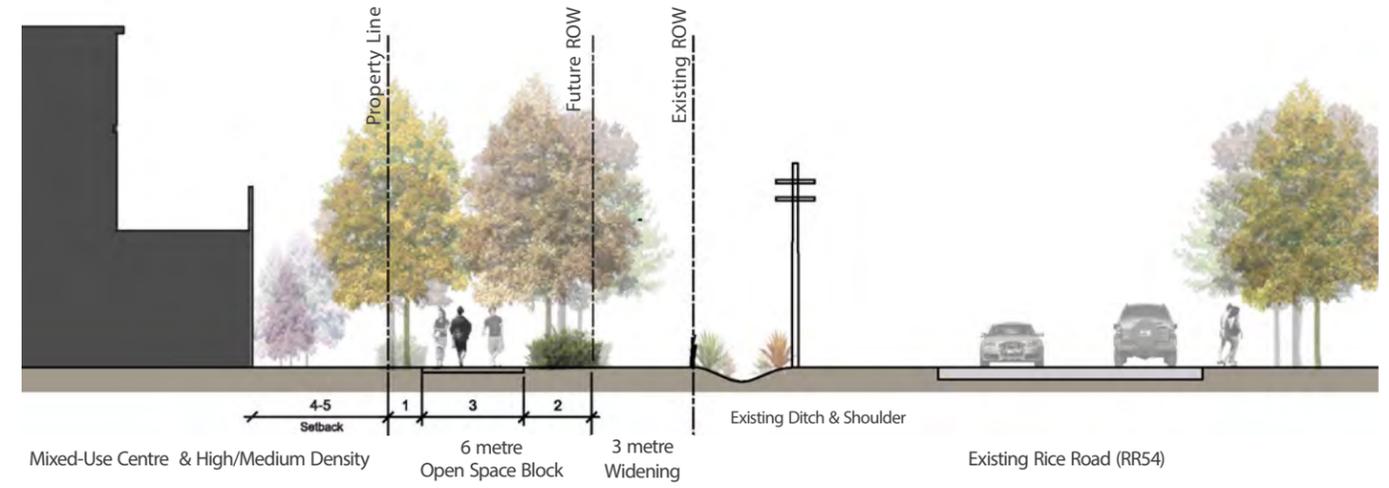
Fencing / Walls



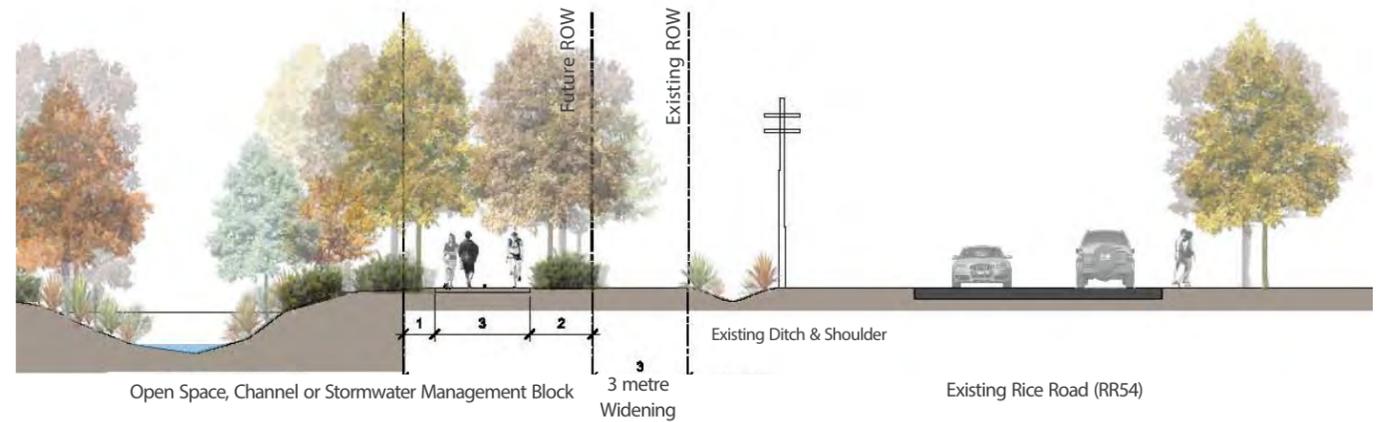
PROPOSED OFF ROAD  
(MULTI-USE) TRAIL  
DEMONSTRATION PLANS &  
URBAN DESIGN GUIDELINES

Land Use Legend

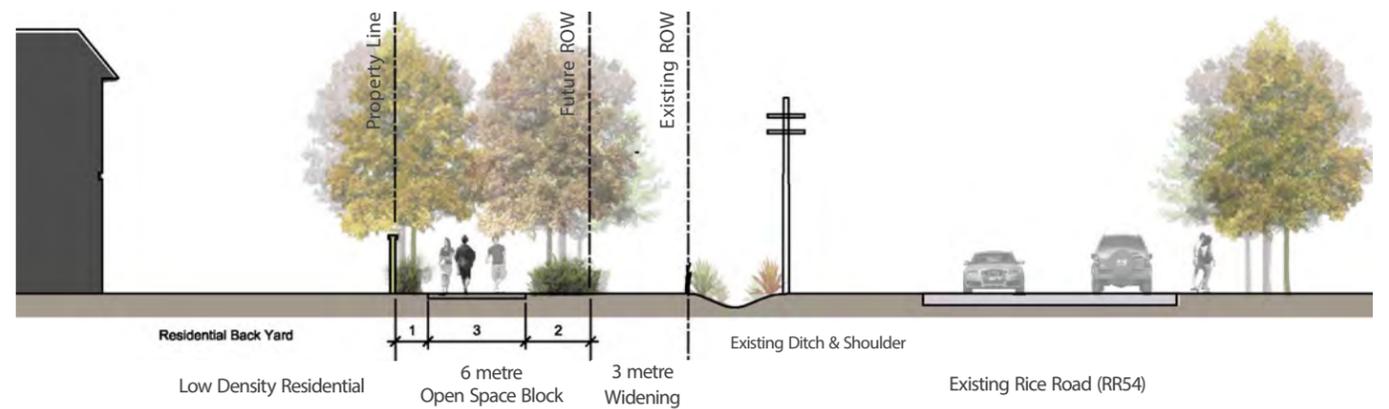
- COMMERCIAL / EMPLOYMENT**
  - COMMERCIAL / EMPLOYMENT
  - MIXED USE
- RESIDENTIAL**
  - HIGH / MEDIUM DENSITY
  - MEDIUM DENSITY
  - LOW DENSITY
  - MEDIUM DENSITY CAP END
  - LOW DENSITY CAP END
- OPEN SPACE**
  - STORMWATER MANAGEMENT POND
  - NEIGHBOURHOOD PARKS
  - PARKETTES LINKAGES
- NATURAL HERITAGE**
  - ENVIRONMENTAL PROTECTION ONE
  - WATERCOURSES



■ Within a 6m Open Space Block adjacent to the Mixed-Use High/Medium Density Area



■ Within Open Space, Watercourses and Stormwater Management Blocks



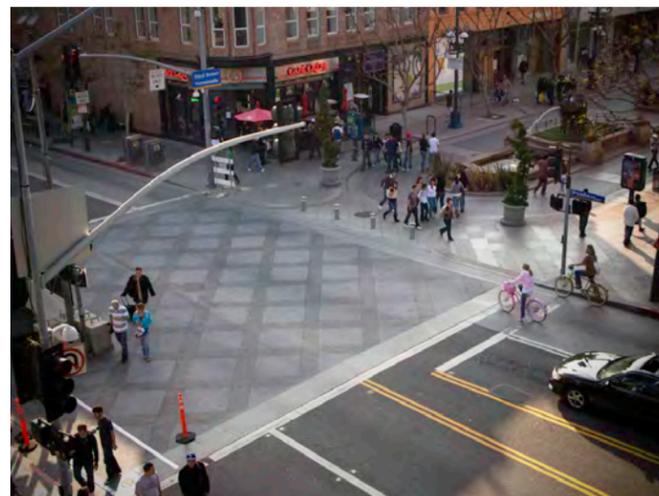
■ Within a 6m Open Space Block adjacent to Low Density Residential

### 4.1.3 CEREMONIAL ROUTE: PEDESTRIAN PROMENADE

Street A is the main Ceremonial Route that will link East Fonthill with the historic downtown. It will be characterized by mixed-uses, residential uses and open space. There are two design conditions for this street, one with a landscaped centre median and one without. It is envisioned that a landscaped centre median would be provided within a 28.0 metre right-of-way, and adjacent to parks, open space and gateway locations (key intersections).

### ROUNDAABOUT TABLETOP (WOONERF)

Two roundabouts are proposed along the Ceremonial Route. These areas will be designed as a raised woonerf intersection with continuous decorative paving throughout the intersection and coordinated landscaping in the adjacent sites.



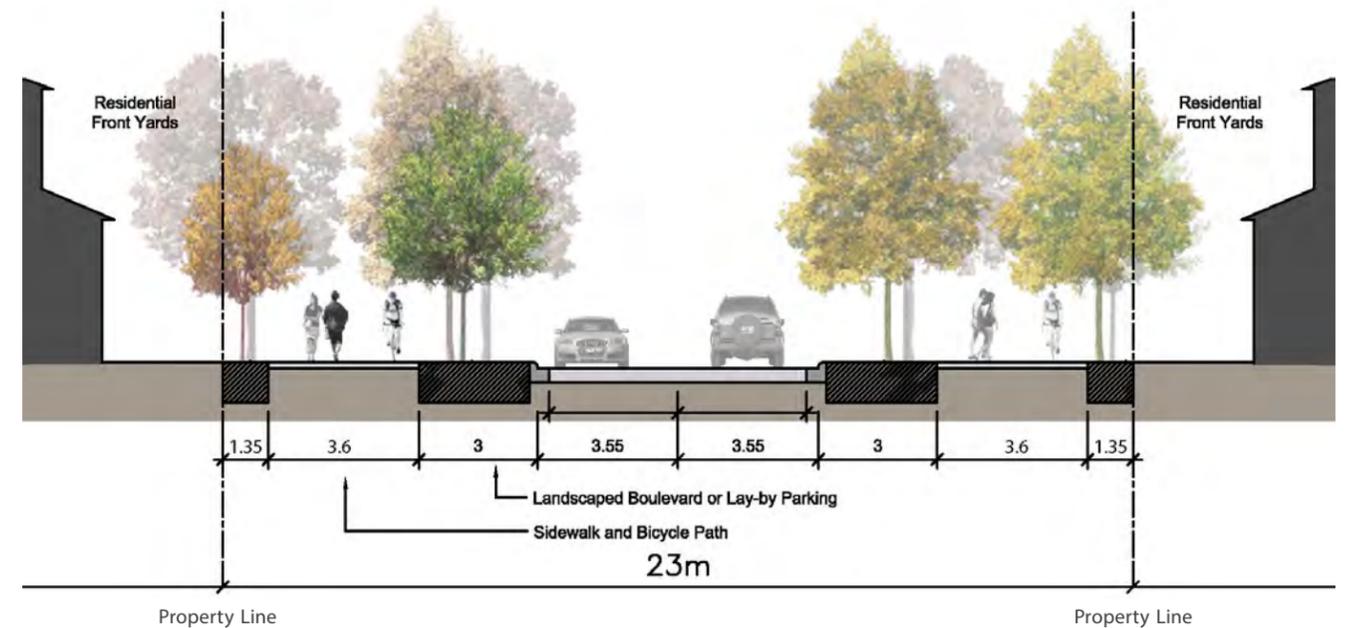
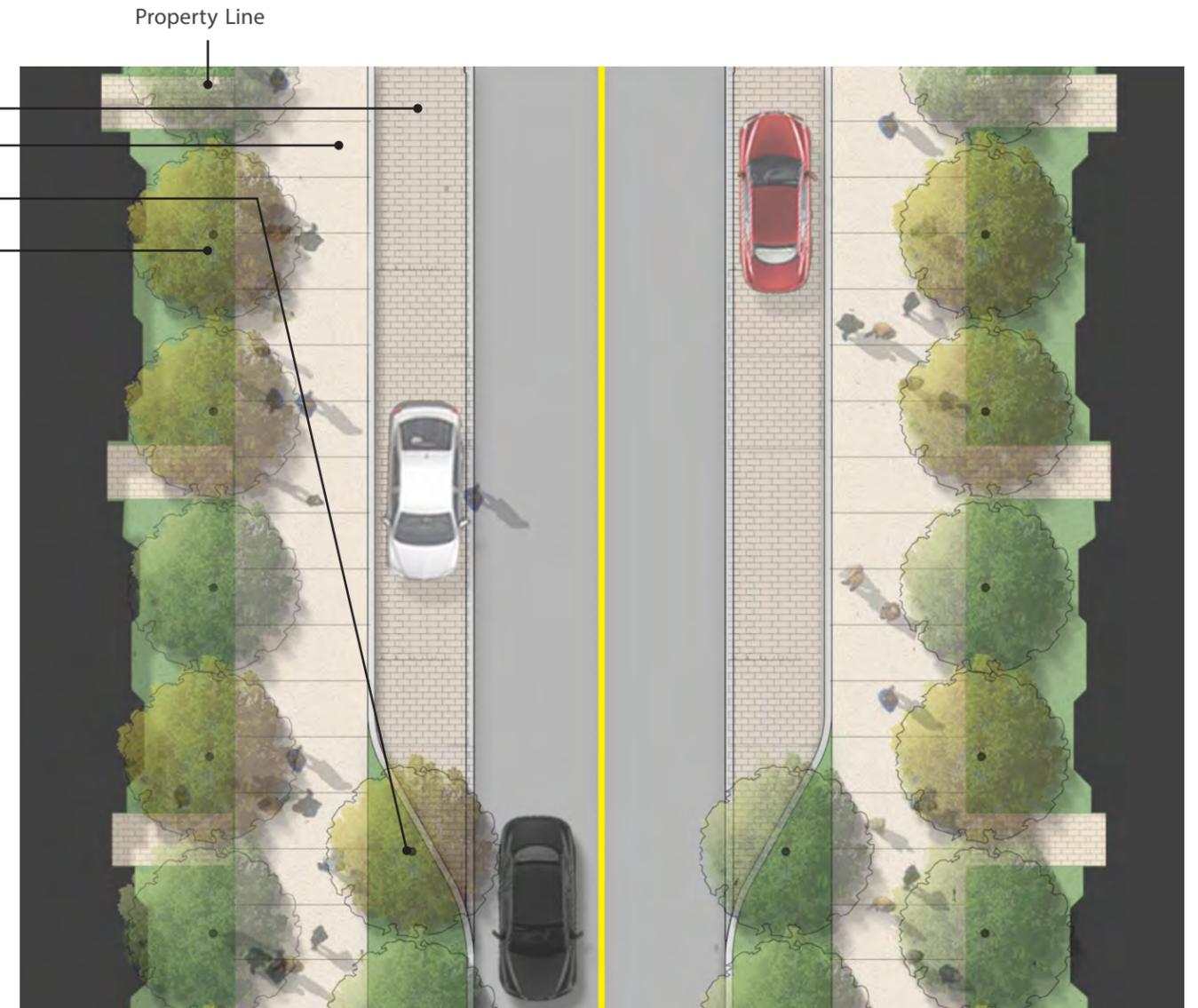
Paving in Shared Intersection



### DESIGN ELEMENTS WITHIN THE 23.0 METRE RIGHT-OF-WAY

- Curb side parking on both sides of the street;
- A 3.6 metre wide urban trail located along the lot line on both sides of the street;
- Urban Trail to be poured in place concrete;
- Street trees that will ultimately arch over the street creating a dramatic parkway setting that appropriately links the two commercial areas and the two major parks; and,
- Large, deciduous canopy trees planted within the boulevard in groups of three, alternating species – Red Maple, Swamp White Oak.

- Lay By Parking
- Urban Trail
- Bump Out / Street Tree
- Street Tree on Property Line



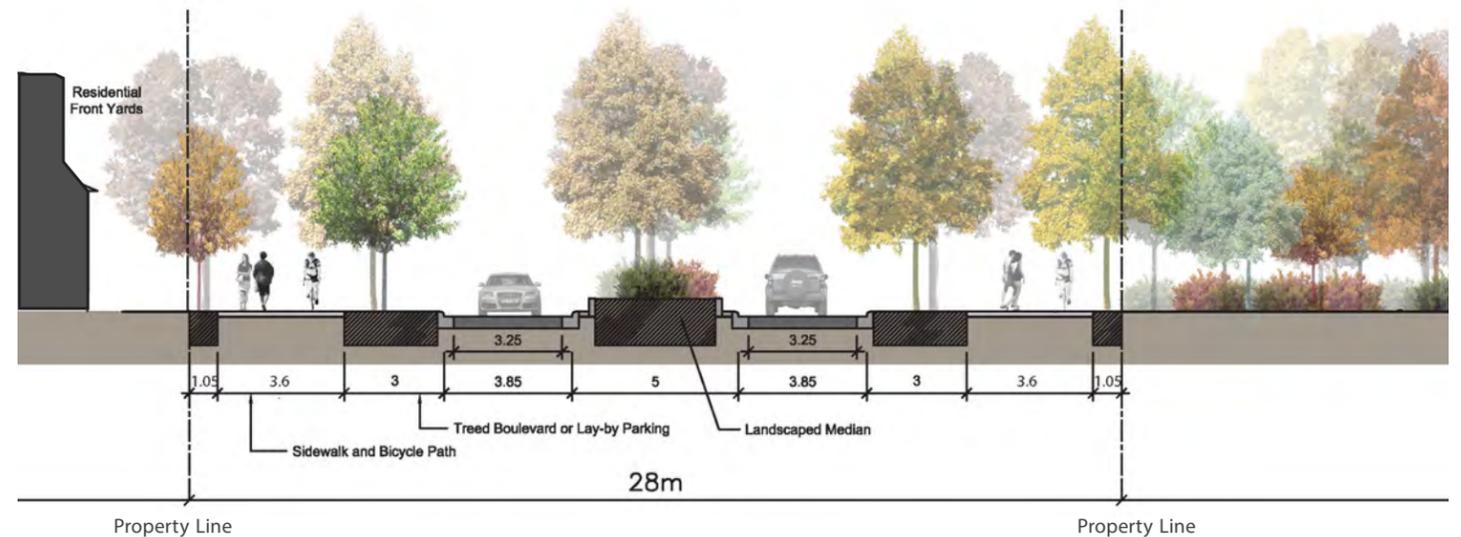
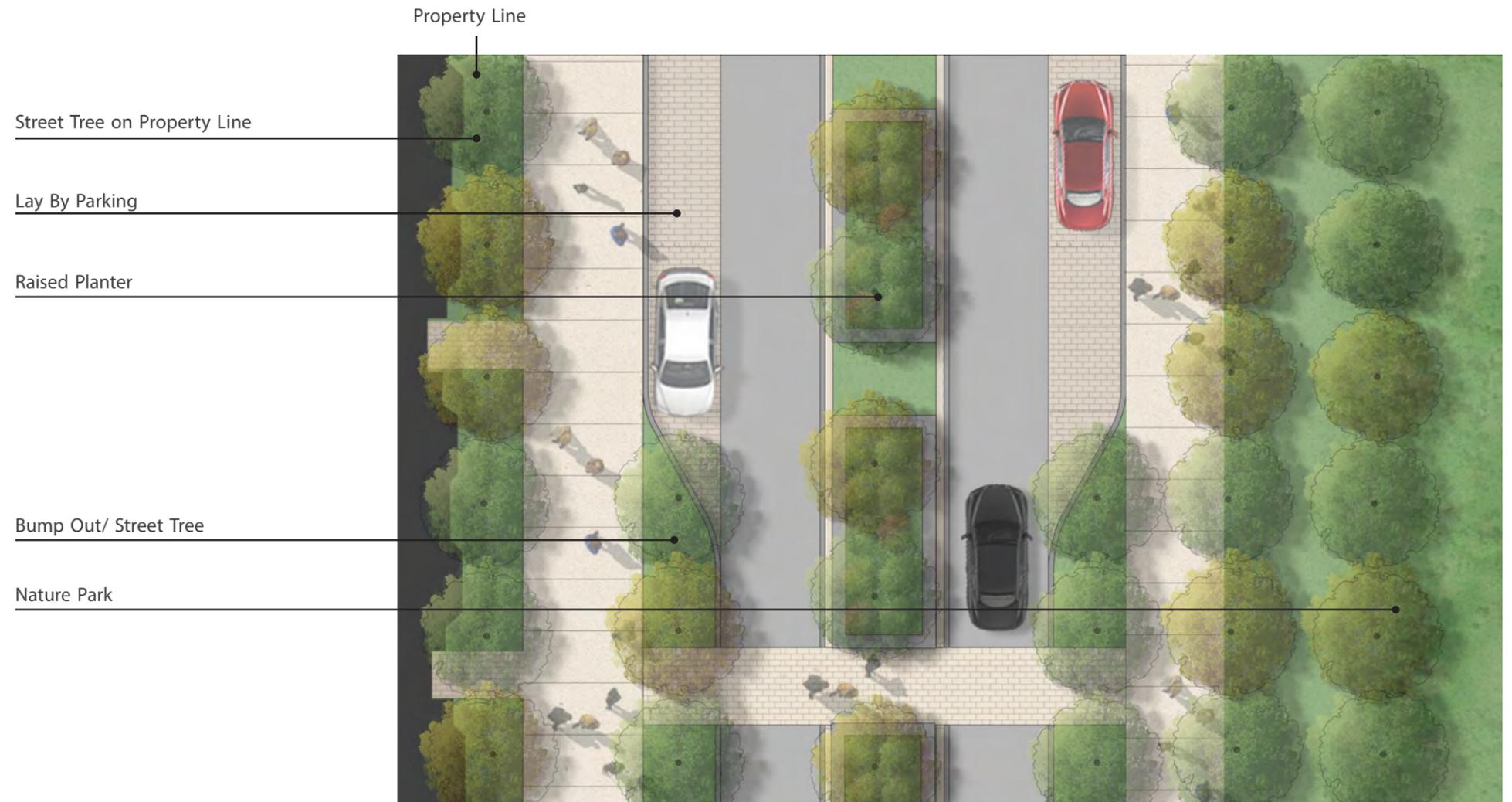
## DESIGN ELEMENTS WITHIN THE 28.0 METRE RIGHT-OF-WAY

A 5.0 metre wide central median containing:

- A raised planter planted with shrubs and a continuous row of deciduous canopy trees – DED White Elm; and,
- Large, deciduous canopy trees planted within the boulevard in groups of three, alternating species – Silver Maple, White Oak. The minimum caliper of trees at planting will be 80 millimetres in diameter.



Landscaped Centre Median



#### 4.1.4 MAIN STREET (WELLSPRING WAY): RIBBONS OF COLOUR

Wellspring Way is the primary welcoming road leading from Regional Road 20 (Hwy 20) into the proposed East Fonthill Mixed Use Centre. It will be characterized by commercial and institutional uses and is envisioned to be an animated, pedestrian-scaled street. This street will also define one of the primary frontages for the planned Community Centre.

Pedestrian Crossing



Tree Grates



Continuous Pedestrian Zone

#### DESIGN ELEMENTS WITHIN THE RIGHT-OF-WAY

- Continuous decorative paving from the back of curb to the right-of-way;
- Large, deciduous canopy trees planted within the paved areas either in pits with tree grates or in raised planters - Black Maple, Freeman's Maple;
- Street trees will be planted a maximum of 8.0 metres apart in line with the trees across the street. They will be planted in continuous tree pits that are a minimum of 2.0 metres wide and that provide a minimum of 15.0 cubic metres of soil per tree. The minimum caliper of trees at planting will be 80 millimetres in diameter;
- Bump-outs, which define the lay-by parking areas, will be sodded and planted with the same species of deciduous canopy trees;
- A complete palette of coordinated street furniture; and,
- The sidewalks will be lit with special pedestrian lighting that supplements the roadway lighting and meets CPTED principles.

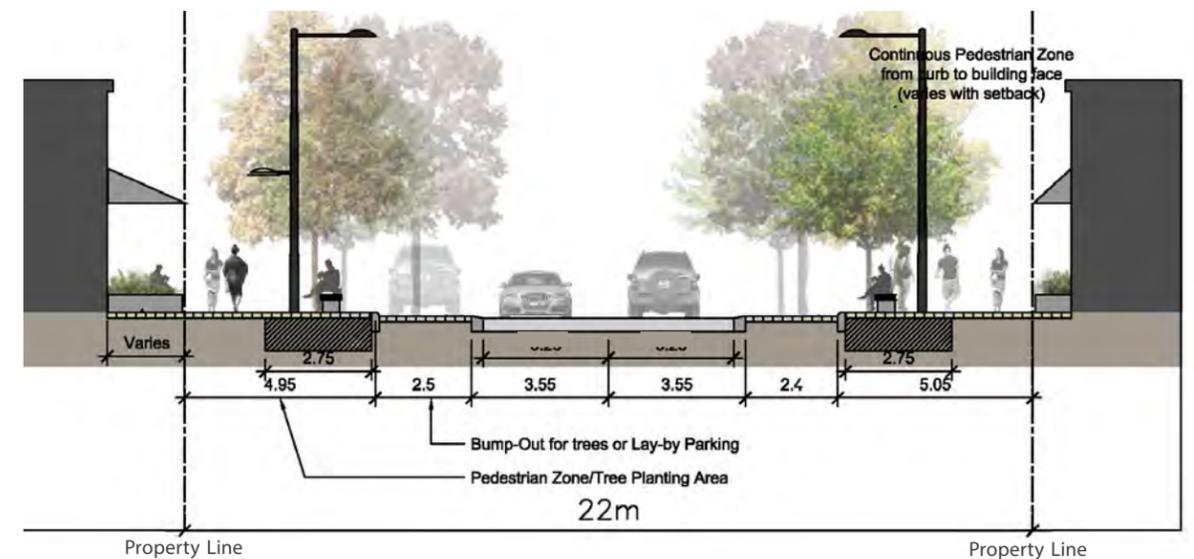
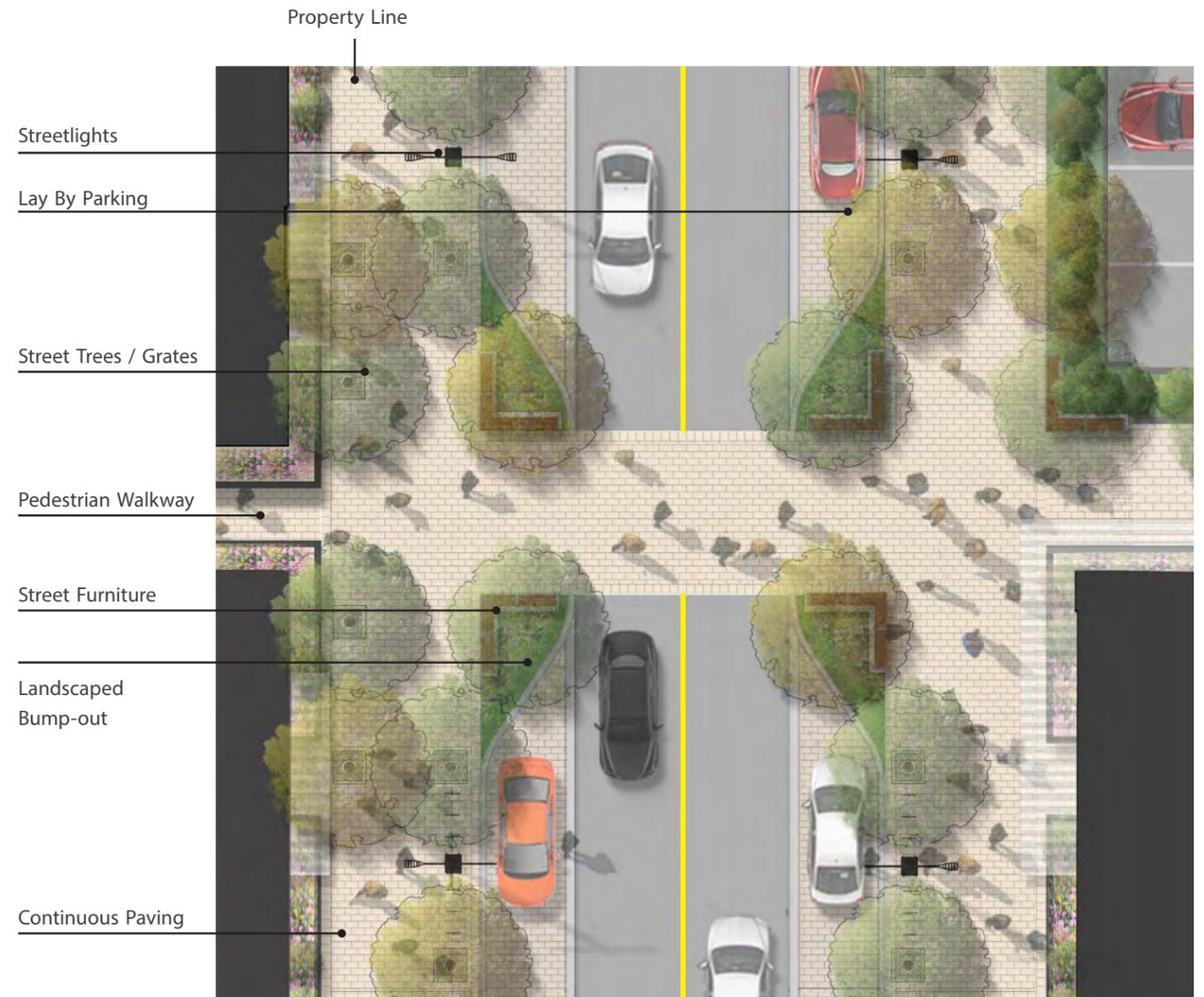
#### DESIGN ELEMENTS WITHIN THE ADJACENT DEVELOPMENT WHERE LOADING AND SERVICING AREAS OCCUR

A minimum 6.0 metres wide landscaped strip to consist of:

- Deciduous canopy trees, shrubs and perennials;
- Planter seatwalls; and,
- Decorative fencing and/or screening.

#### DESIGN ELEMENTS WITHIN THE ADJACENT DEVELOPMENT WHERE BUILDINGS OCCUR

- Continuous decorative paving from the right-of-way to the building wall; and,
- Deciduous canopy trees in pits with tree grates or raised planters, where space permits planters.



## 4.1.5 COLLECTOR STREET C: RIBBONS OF COLOUR

Collector Street C is the main east-west collector service road linking the Mixed Use Centre. It leads into the Main Urban Square and provides access to a number of development sites as well as providing a main urban trail linkage between Rice Road (RR54) and the Greenway.

The typical Collector Street right-of-way will be 22.0 metres in width with one travel lane in each direction.

### DESIGN ELEMENTS WITHIN THE RIGHT-OF-WAY

- A 3.6 metre wide urban trail located along the lot line on both sides of the street;
- The urban trail will be poured in place concrete;
- A 3.0 to 4.0 metre wide planting zone will be provided adjacent to the sidewalk for street trees. That planting zone will provide a minimum of 15.0 cubic metres of topsoil in a continuous trench for street trees planted on minimum 8.0 metre wide centres; and,
- Large, deciduous canopy trees planted within the boulevard in groups of three, alternating species of maple – Black Maple, Freeman’s Maple. The minimum caliper of trees at planting will be 80 millimetres in diameter.

### DESIGN ELEMENTS WITHIN THE ADJACENT DEVELOPMENT WHERE LOADING AND SERVICING AREAS OCCUR

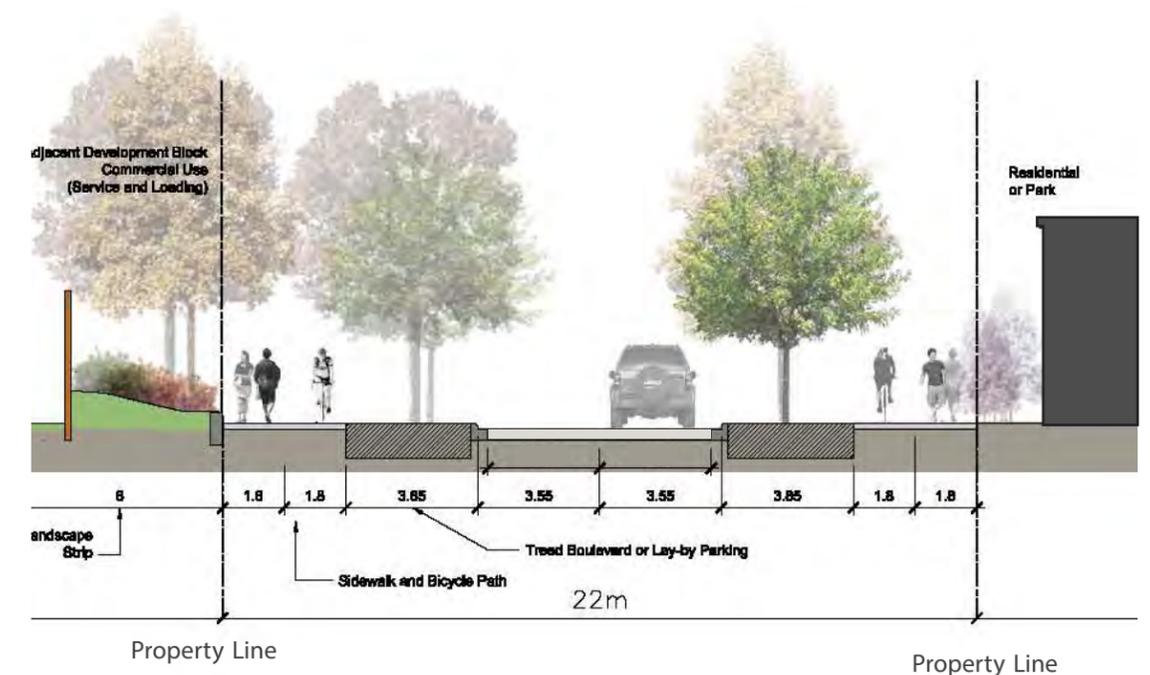
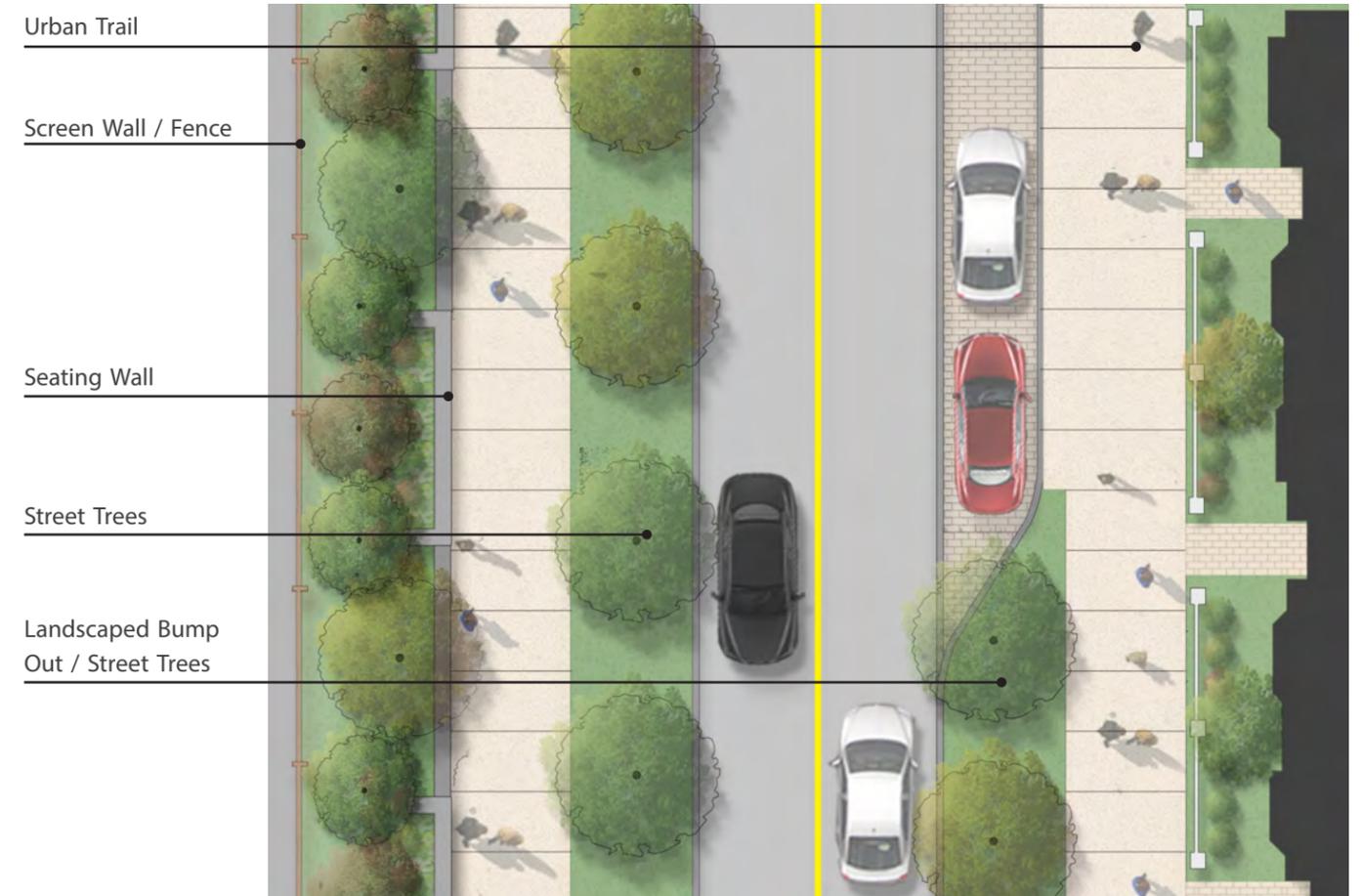
A minimum 6.0 metre wide landscaped strip to consist of:

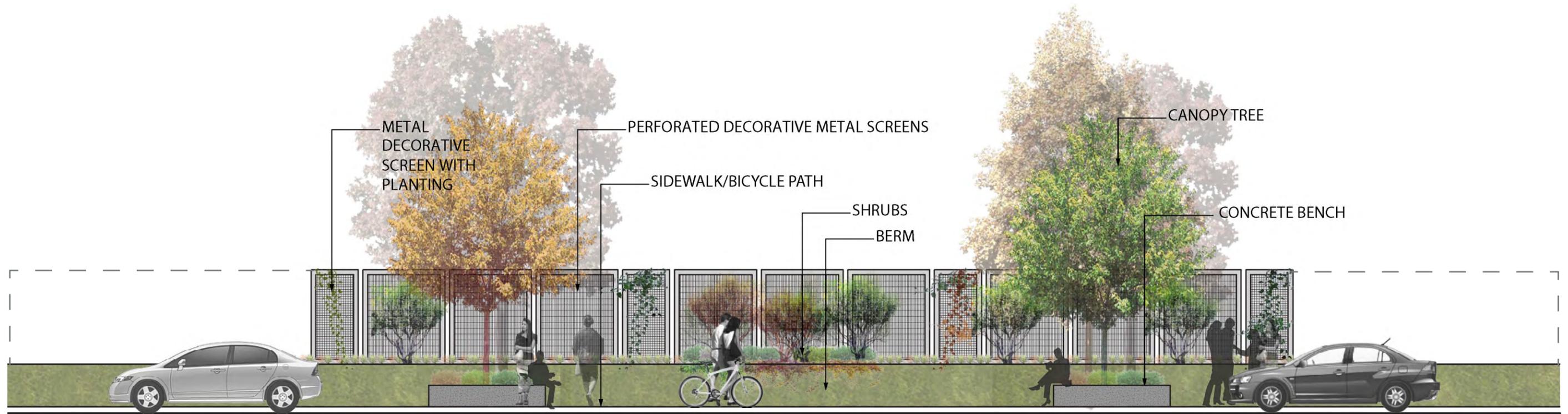
- Deciduous canopy trees, shrubs and perennials;
- Planter seatwalls; and,
- Decorative fencing and/or screening.

Seating



Screen Wall / Fence





Collector Street C - Elevation



Vine Planting on decorative Wall/Fence



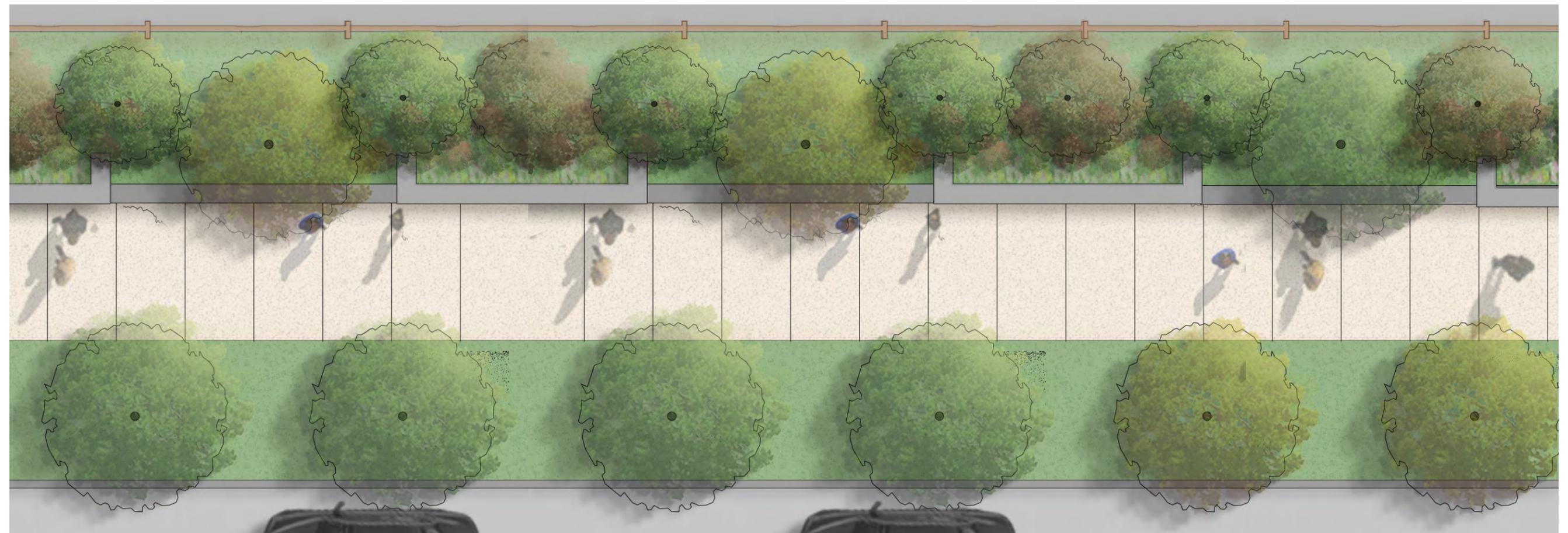
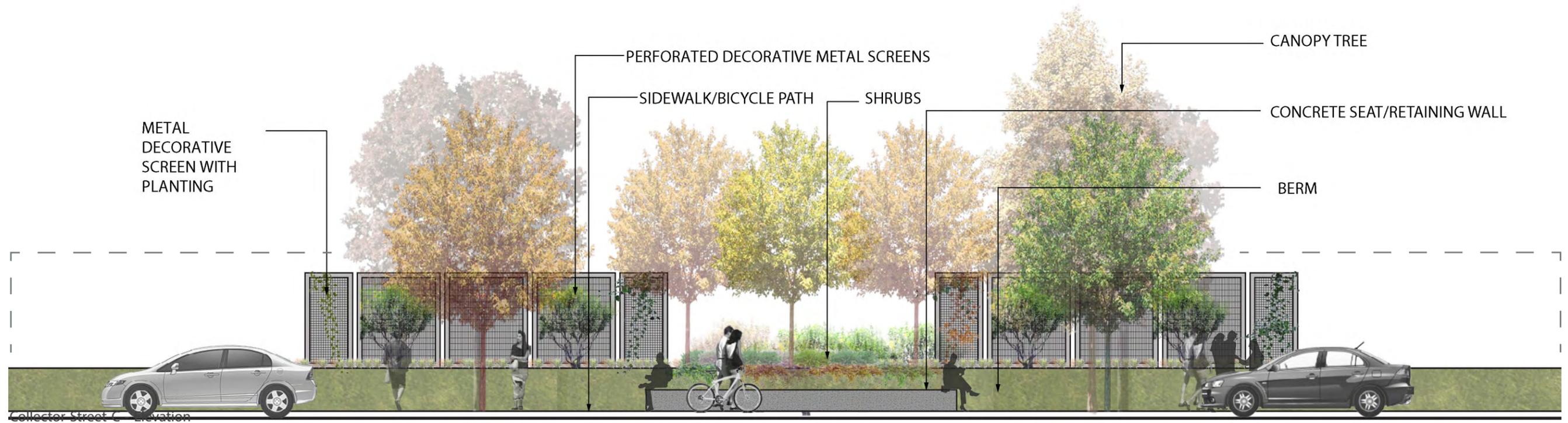
Perforated decorative Metal Screen Wall



Perforated decorative Metal Screen Wall



Concrete Bench



## 4.1.6 LOCAL GREENWAY

The Local Greenway is envisioned to become an ecological corridor providing a linkage from Regional Road 20 (Hwy 20) south to the natural heritage features located at centrally within the East Fonthill Secondary Plan Area. It is intended to provide a major trail connection from the proposed Mixed Use Centre, southward with connections to the Steve Bauer trail, and the East Fonthill trails network. It will be characterized by plantings of trees, shrubs and perennials within the boulevard to form a continuous tree canopy and potential wildlife corridor.

### DESIGN ELEMENTS WITHIN THE RIGHT-OF-WAY

- A 3.6 metre wide urban trail located along the lot line on both sides of the street;
- Large, deciduous canopy trees planted within the boulevard, alternating species of – Black Gum, Tulip Tree and Coffee Tree. The minimum caliper of trees at planting will be 80 millimetres in diameter;
- Street trees will be planted at a maximum of 8 metre on centre in a continuous soil trench along the edge of the right-of-way to take advantage of soil volumes on adjacent properties. The intent is to provide a minimum of 15 cubic metres per tree of topsoil to achieve full maturity; and,
- Small trees, shrubs and perennials will be planted in the median. 1.8 metre wide poured in place concrete sidewalks will be positioned adjacent to the curb edge.



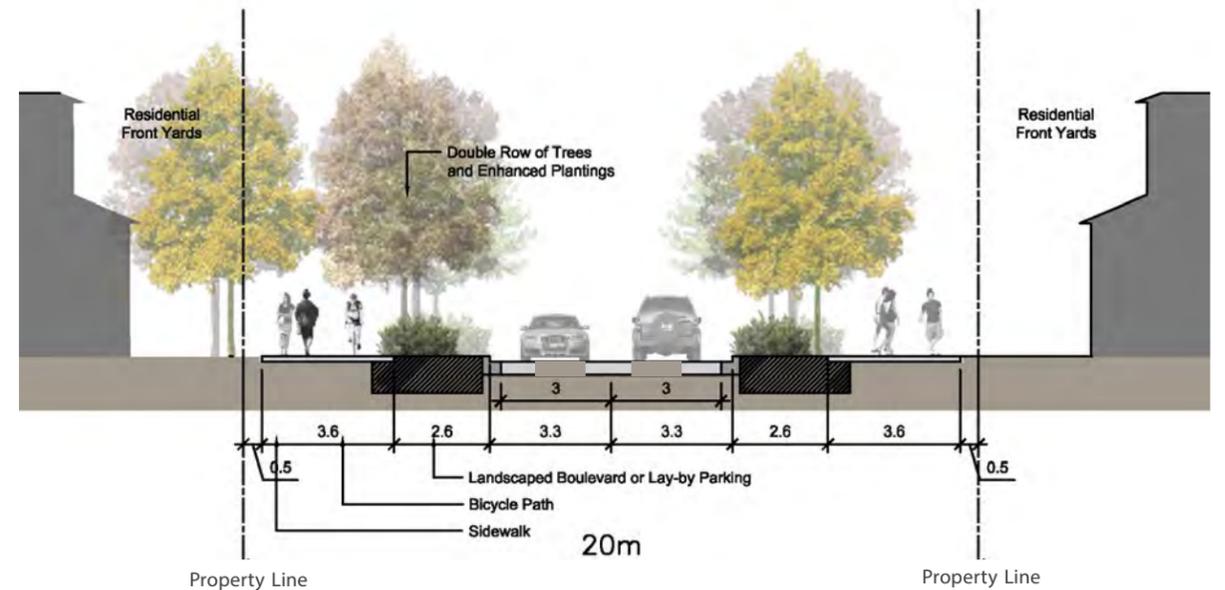
Landscaped Boulevard

Urban Trail

Street Trees

Naturalized  
Landscaped  
Boulevard

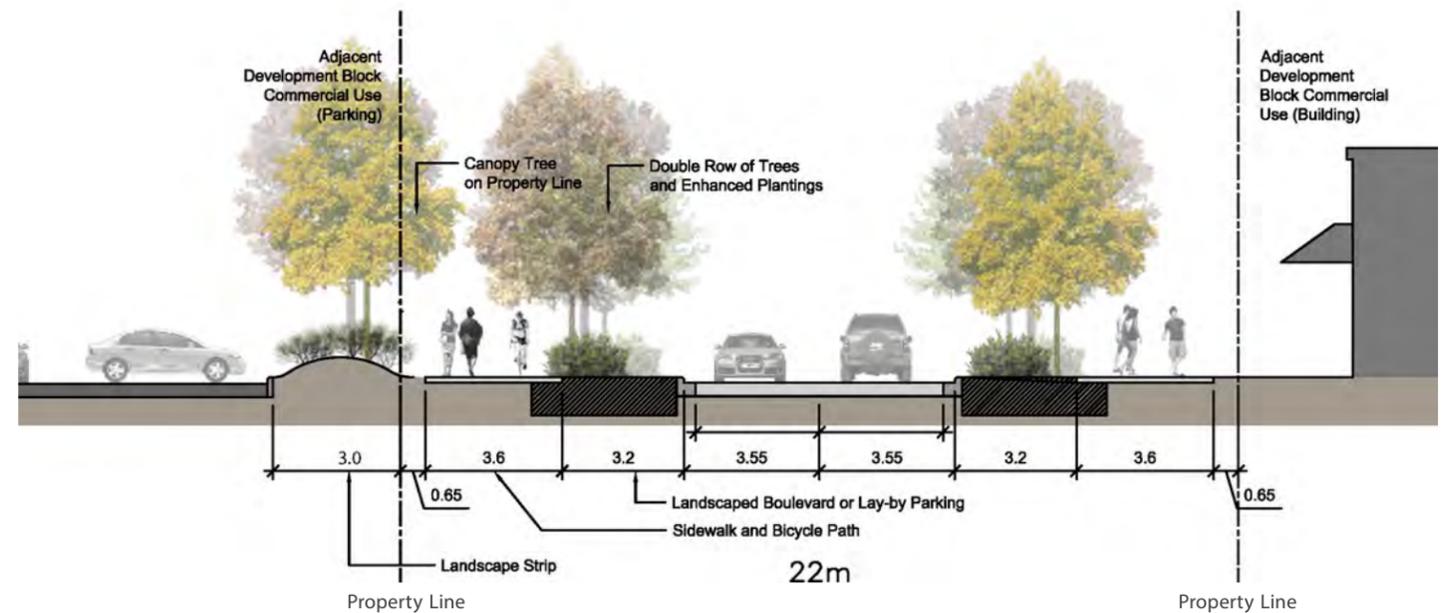
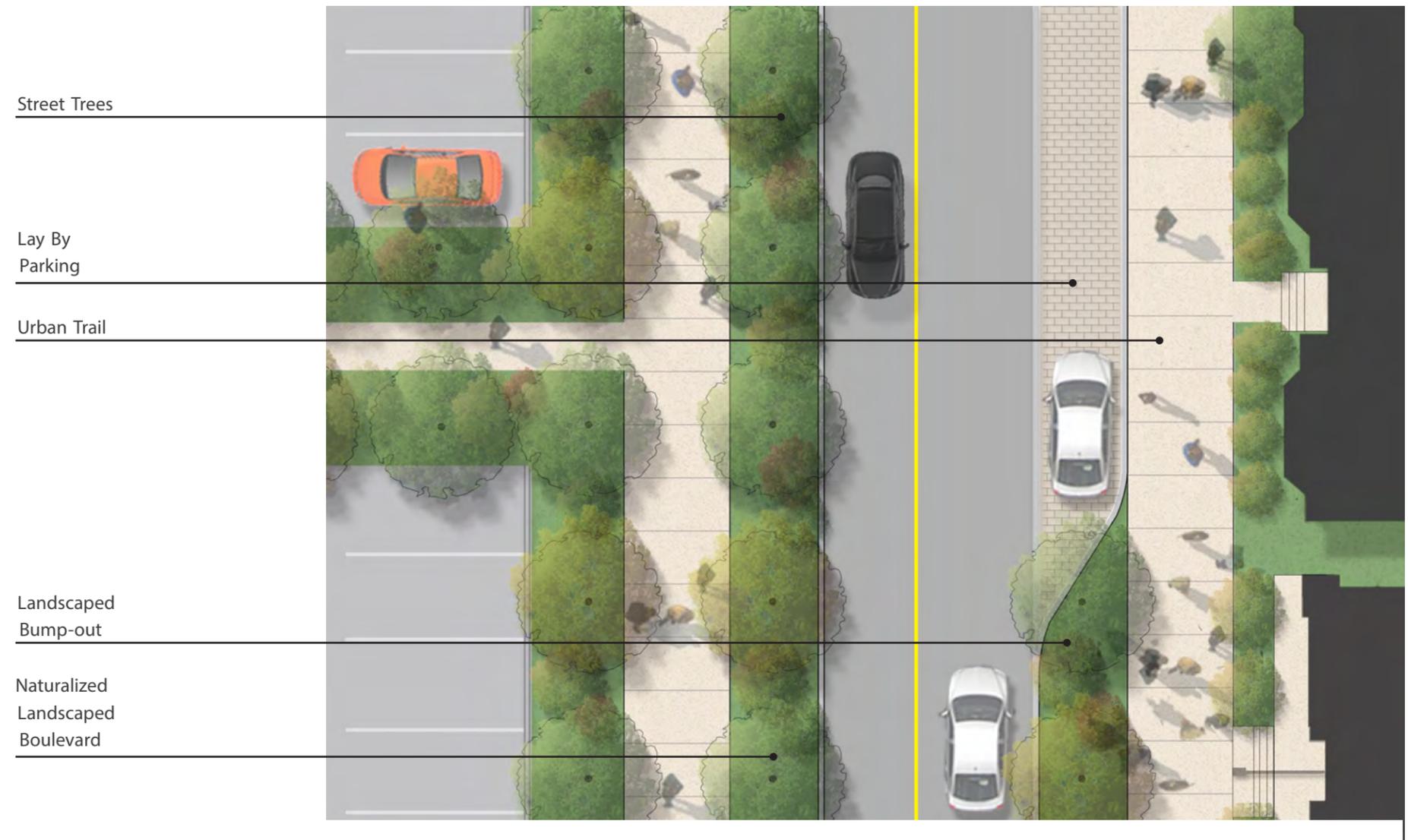
Landscaped  
Bump Out



## DESIGN ELEMENTS FOR THE ADJACENT DEVELOPMENT WHERE PARKING AREAS OCCUR

A minimum 3.0 metre wide landscaped strip to consist of:

- Deciduous canopy trees, shrubs and perennials. The minimum caliper of trees at planting will be 80 millimetres in diameter.



## 4.2 OPEN SPACE COMPONENTS

### 4.2.1 MAIN URBAN SQUARE: A COLOUR EXPLOSION

The Main Urban Square is an important central focus within the proposed East Fonthill Mixed Use Centre. It is envisioned to become a dynamic, animated and pedestrian focused public space where organized community events and informal gathering will take place. To support these objectives, the development of the streets and space, as well as the surrounding lands should enhance both the function and the aesthetics of the square.

It will be finished in high quality material including unit paving that will extend across the intersection to tie all four corners of the space together. The space will be lit with pedestrian scale lighting and be fitted with the necessary extra power supply for events. Lighting should be designed having regard for the Town's CPTED principles and lighting standards.

#### DESIGN ELEMENTS WITHIN THE STREET RIGHT-OF-WAY

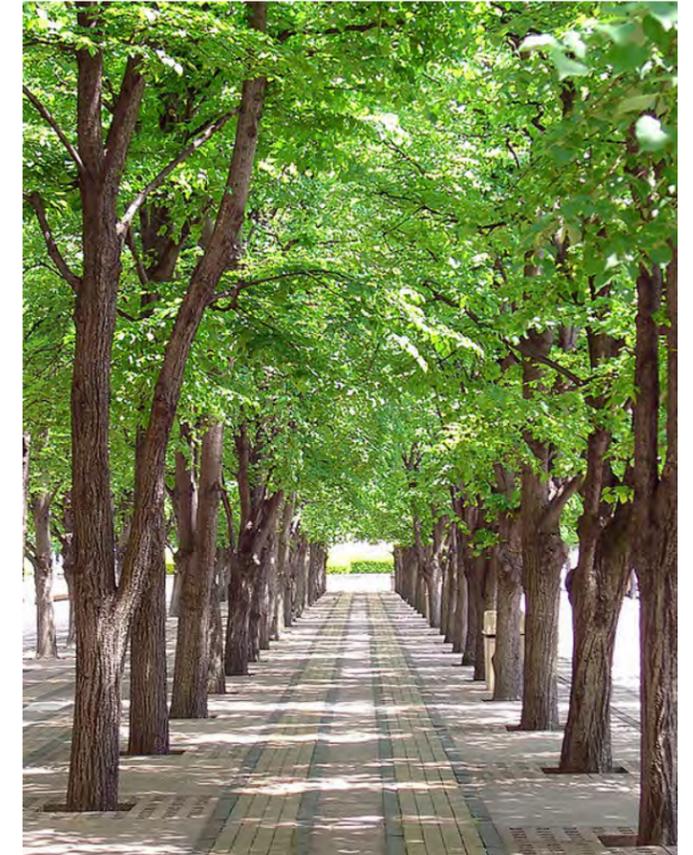
- Pedestrian pergolas;
- Street furniture; and,
- Deciduous canopy trees – Black Maple, Freeman's Maple. The minimum caliper of trees at planting will be 80 millimetres in diameter.

#### DESIGN ELEMENTS WITHIN THE ADJACENT CORNERS

- Pedestrian pergolas;
- Raised planters;
- Street furniture; and,
- Deciduous canopy trees – Paperbark Maple, Tatarian Maple, Three Flower Maple. The minimum caliper of trees at planting will be 80 millimetres in diameter.

#### BUILT FORM GUIDELINES

- Primary building facades and main entrances should be oriented to the square;
- Surface parking is not permitted adjacent to the square;
- Storage and service areas are not permitted on, or adjacent to the square;
- Landscape design shall be coordinated, consistent, and represent a continuation of the urban square and the adjacent sites;
- Building setbacks should allow for the extension of adjacent uses such as patios, gardens and seating areas overlooking the square (refer to Zoning for specific requirements);
- Building forms, materials and colours, shall complement the design of the square; and
- Buildings should consider incorporating architectural elements that enhance the function and design of the square (i.e. continuous covered canopies, porticoes, colonnades).



Gathering Area



Commercial Site



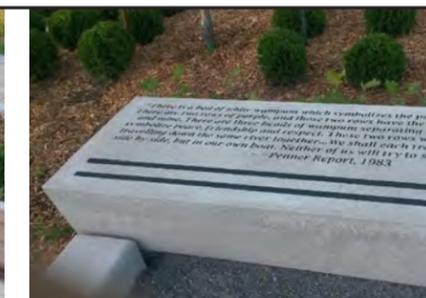
## DESIGN ELEMENTS WITHIN THE SPACE

- A central lawn area;
- A skating trail;
- A water feature;
- A raised pavilion;
- Pedestrian pergolas;
- Screen walls/fencing;
- Seating areas;
- Street furniture; and,
- Deciduous canopy trees – Black Maple, Sugar Maple, Red Maple, Paperbark Maple, Tartarian Maple, Three Flower Maple. The minimum caliper of trees at planting will be 80 millimetres in diameter.

Central Lawn



Seating Areas



Raised Pavilion



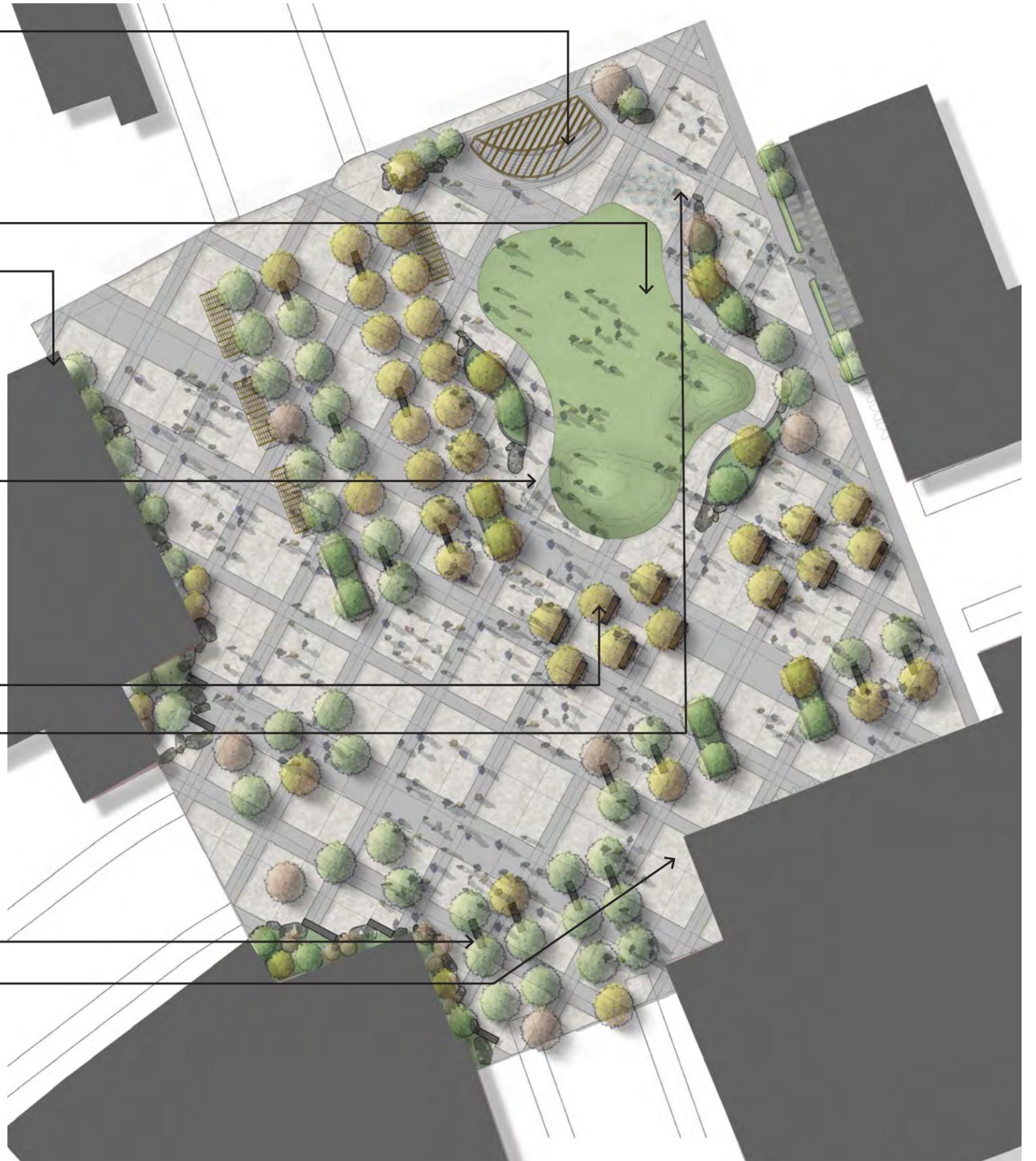
Commercial Site



Water Element



Plaza Paving



Screen Walls / Fencing



Raised Pavilion



Seating Area



Water Element



Plaza Paving



Plaza Tree Planting



North-South Section Through Lawn



East-West Section

## 4.2.2 NATURE PARK

The Nature Park is contiguous with the South Pond and the protected stream channel, and effectively functions as an extension of these spaces. At the same time, these three components combine to form a significant central open space for East Fonthill that is characterized by large open areas, naturalized planting and amenities to accommodate a wide range of primarily passive recreation, including key trail connections.

### DESIGN ELEMENTS

- A system of pathways, 3.0 metres wide, paved with asphalt;
- A children's adventure playground;
- Adult fitness areas;
- Shaded seating areas;
- Pedestrian pathway lighting;
- Open play areas;
- Naturalized and interpretive areas adjacent to the South Pond and protected stream channel; and,
- Large, deciduous canopy trees, alternating species of – Ironwood, Muscledwood, Chinquapin Oak, Dward Chinquapin Oak, Pin Oak, Bur Oak, Cucumber Magnolia, Yellowwood, Eastern Redbud.

Trails



Open Play Area



Children's Play



Walkways



Adult Fitness



### 4.2.3 GATEWAY POND

The Gateway Pond is located at the most prominent location at the corner of Regional Road 20 (Hwy 20) and Rice Road (RR54). It represents an opportunity for the Town to develop a significant gateway element, integrated with the design of the stormwater management pond, to promote Town identity.



### DESIGN ELEMENTS

- A 4.0 metre wide boardwalk linking the urban trail along Regional Road 20 (Hwy 20) and the multi-use trail along Rice Road (RR54);
- A pedestrian area overlooking the pond;
- Connections to the adjacent seniors residence site;
- Town of Pelham signage located at the corner;
- Opportunities for public art; and,
- Naturalized planting.



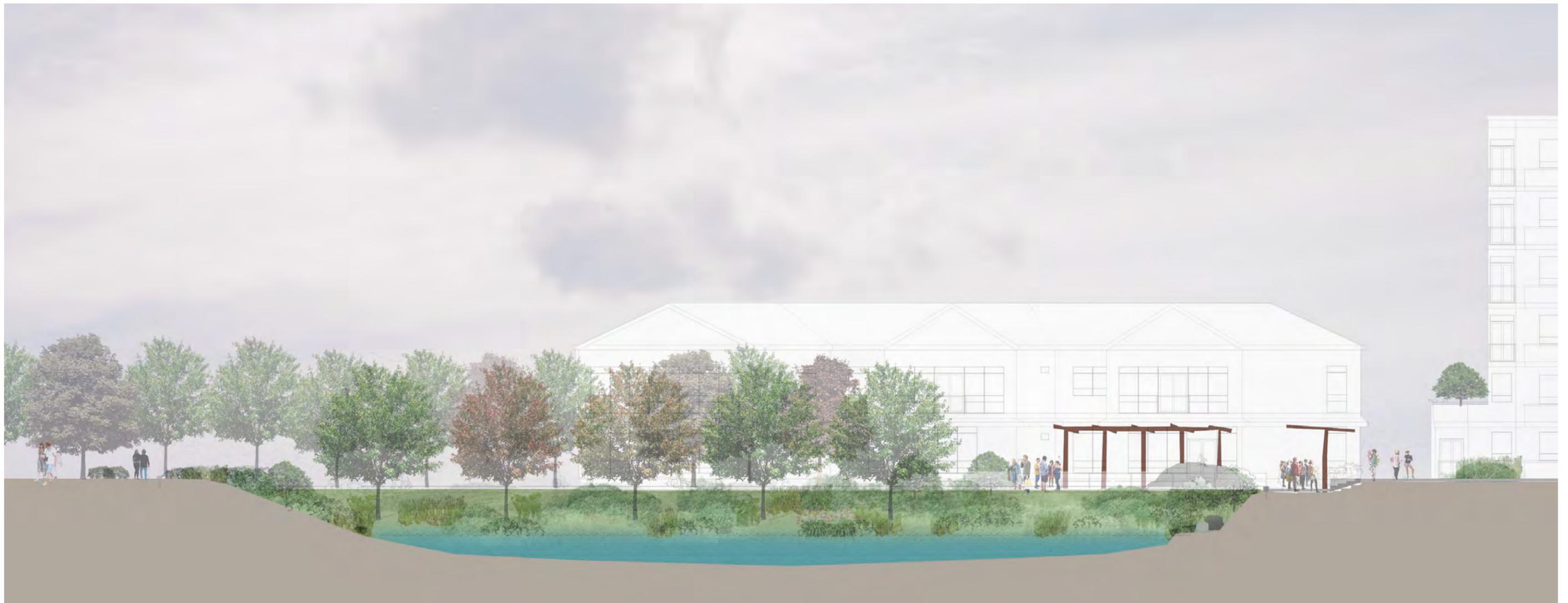
View of Gateway Park / North Stormwater Pond from RR20 (Hwy 20) and Rice Road (RR54)



Gateway Park / North Stormwater Pond Birdseye View



Gateway Park / North Stormwater Pond Elevation



## 4.2.4 SOUTH POND

The South Pond is located at the corner of the Ceremonial Route and Main Street (Wellspring Way), at the most southern edge of the East Fonthill Mixed Use Centre. It represents an opportunity for the Town to connect this public space with nearby trails, integrated with the design of the stormwater management pond. The location is convenient for pedestrians as it is adjacent to Nature Park and a trail system.

### DESIGN ELEMENTS

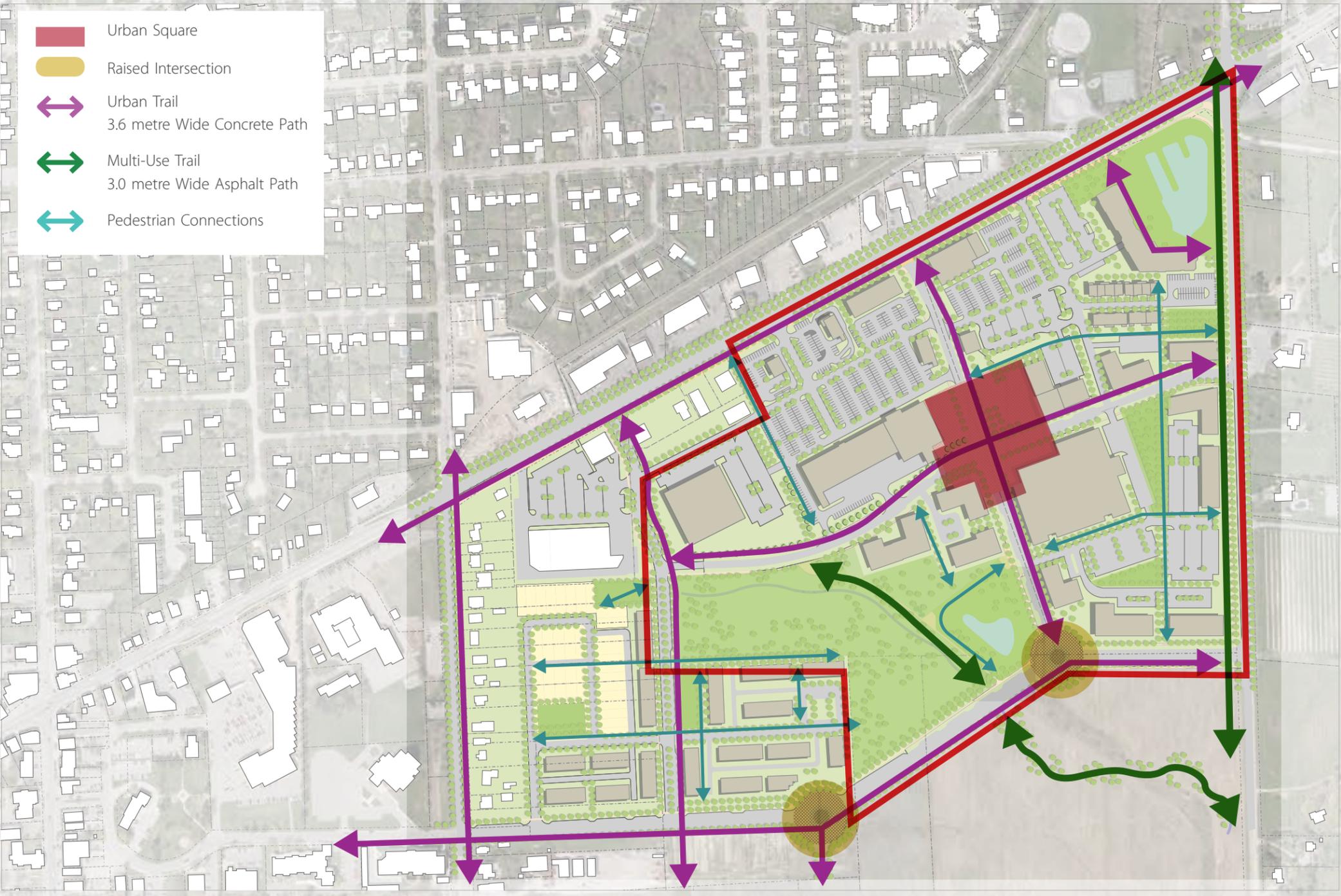
- Pedestrian nodes along the Wellspring Way street interface, including seating, pedestrian lighting and interpretive signage;
- A pedestrian node at the connection to the channel;
- Opportunities for public art; and,
- Naturalized planting.



### Seating



### 4.3 TRAIL SYSTEM



## URBAN TRAILS

- Curb-cuts shall be provided at all road crossings to improve accessibility.

## MULTI-USE TRAILS

- Located within open space and trail blocks contiguous with Rice Road (RR54);
- Located within the channel blocks;
- Grading of creeks, stormwater management facilities and drainage swales shall accommodate a maximum slope of 5% and 2-3% cross slope for trails; and,
- Curb-cuts shall be provided at all road crossings to improve accessibility.

Trail Paving



Seating



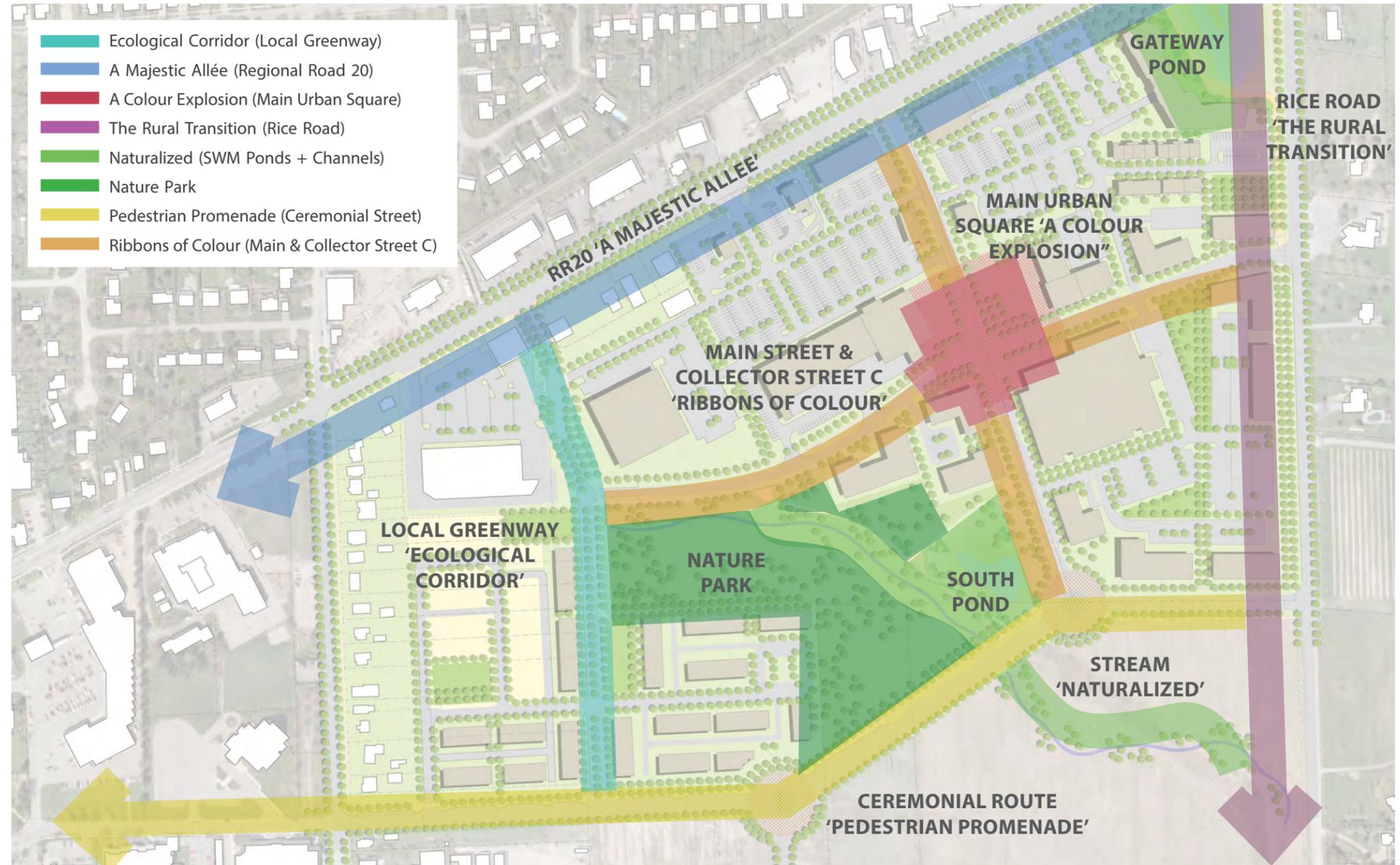
## 4.4 LANDSCAPE ELEMENTS

### 4.4.1 URBAN TREE CANOPY

The proposed urban tree canopy contains a diverse variety of recommended tree species. The species selected for each of the main areas and streets in East Fonhill connects them with local greenways and existing natural areas. The overall urban tree canopy will increase with the addition of these new trees to the area.

#### PRINCIPLES

- Enhance diversity/resiliency;
- Incorporate native species;
- Celebrate Natural Heritage;
- Enhance the natural environment (naturalized);
- Reinforce special places;
- Create Tree Lined Street; and,
- Create Connected Canopies.



#### ECOLOGICAL CORRIDOR (LOCAL GREENWAY)



Black Gum



Tulip Tree



Coffee Tree

#### A MAJESTIC ALLEE (REGIONAL ROAD 20)



Red Oak



Shingle Oak



Chinquapin Oak

**A COLOUR EXPLOSION (MAIN URBAN SQUARE)**



Black Maple



Sugar Maple



Red Maple \*



Paperbark Maple



Tatarian Maple



Three Flower Maple

**THE RURAL TRANSITION (RICE ROAD)**



Trembling Aspen



Basswood

**NATURALIZED (SWM PONDS + CHANNELS) SEE OTHER SPECIES MARKED WITH \***



Common Hackberry



Tulip Tree



Wild Crabapple



Black Gum



Trembling Aspen



Black Cherry



Pin Oak



Shumard Oak



Musclewood

**NATURE PARK**



Ironwood \*



Musclewood



Chinquapin Oak



Dwarf Chinquapin Oak



Pin Oak



Bur Oak



Cucumber Magnolia



Yellowwood



Eastern Redbud

**PEDESTRIAN PROMENADE (CEREMONIAL STREET)**



Silver Maple \*



White Oak \*



DED White Elm (median)



Red Maple \*



Swamp White Oak

**RIBBONS OF COLOUR (MAIN & COLLECTOR STREET C)**



Black Maple



Freeman's Maple

### 4.4.2 PUBLIC ART IDEAS

Public art comes in many shapes, sizes and forms within the public realm, and it generates discussion within the community. Public art is a reflection of the artist's interpretation of the space and typically has meaning, whether it is historical or place-specific. It can also be designed to be interactive, which encourages the public to connect with the art.



### 4.4.3 COORDINATED STREET FURNITURE

A Coordinated Street Furniture Program is beneficial throughout East Fonthill where public realm components are required. It contributes to a well-designed pedestrian-oriented streetscape and adds to the public realm by harmonizing the materials and placement. The following amenities should be included as part of coordinated street furniture:

- Bicycle lock-ups;
- Tree grates;
- Waste/recycling receptacles; and,
- Seating.

Bicycle Lock-ups



Tree Grates



Waste/Recycling Receptacles



Seating



#### 4.4.4 LIGHTING STRATEGY

The Lighting Strategy for the proposed East Fonthill Mixed Use Centre serves as a reference for future lighting upgrades to enhance public safety, security and wayfinding, while creating a dynamic ambiance for public gatherings and night time events.

Illumination levels are to conform to the Town of Pelham guidelines, Lighting Policy Standards and CPTED principles. It is envisioned that the Lighting Strategy will largely be incorporated into the Town's development and planning policy, and follow the principles adopted by a growing list of cities including Paris, Brussels, Toronto, London, Edinburgh, Milan, Rome, Quebec City and Singapore. Principles to include:

- Lighting must use architectural concrete bases; and,
- Reflective baird on base is required if within 3 metres of vehicular traffic.

#### THE SPECIAL CHARACTER OF EAST FONTHILL

Neo-traditional to contemporary describes the lighting character desired for the proposed Mixed Use Centre Streetscapes, Open Space and Urban Square. The landscape lighting placed along the public spaces will gradually connect the Mixed Use Centre to Downtown Fonthill, and the rest of the community, tracing a luminous urban fabric as background for the night-time landscape.

#### CURRENT CONDITIONS AT NIGHT

The character of the existing lighting can best be describes as utilitarian suburban, servicing regional roads, commercial lot parking and driveway, and residential streets.

#### GENERAL PRINCIPLES AND OBJECTIVES

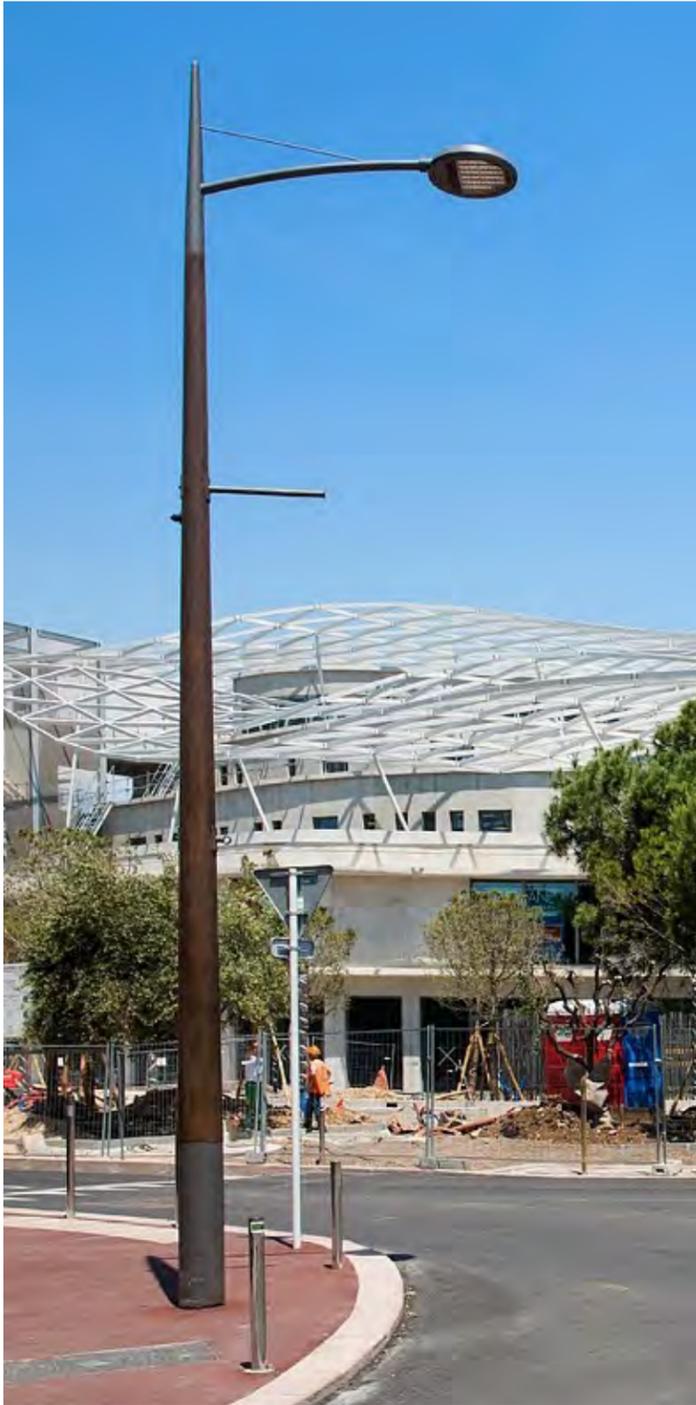
The Lighting Strategy is based on the following general principles:

- To provide guidelines for safe, convenient, and efficient lighting for pedestrians, vehicles and property;
- To integrate the lighting equipment into the streetscape and landscape, enhancing security and wayfinding for the proposed streetscapes and Main Urban Square;
- To educate public agencies, making them aware of the visual and psychological effects of urban lighting;
- To develop a common lighting narrative allowing effective communication between various public agencies involved in urban lighting;
- To bring public and private initiatives together in a unified lighting scheme;
- To develop a guiding plan which supports commerce, enhances landscape architecture and creates a dynamic, interactive points of interest;
- To highlight the form of the Urban Square at night, and to help visualize the lighting applications;
- To prioritize requirements for future design development;
- To provide an night-time identity, highlighting the landscape features and public spaces; and,
- To create a welcoming night-time atmosphere and a distinguishing character to the pedestrian walkways and public spaces.

#### GENERAL GUIDELINES

- The entire length of the streetscapes should be illuminated in warm white wide spectrum light, consistent with natural evening light;
- All lighting and lighting fixtures should be integrated and designed to enhance the visual and architectural impacts of the site;
- Lighting is to be directed away from adjacent living spaces and streets;
- Lighting design and installation shall ensure that illumination needs are accomplished without intruding on adjacent properties. The illumination is uniform throughout the site;
- Architectural and decorative light or light fixtures shall be employed on all sites abutting Regional Road 20 (Hwy 20) East of Pelham Street and collector roads within the proposed Mixed Use Centre. Architectural and decorative lighting shall strive to achieve accentuation of features, vegetation, and signage, as well as create through light, an interesting and aesthetic built mass;
- Energy efficient LED wide spectrum lighting to be used through-out; and,
- All lighting and lighting fixtures should be integrated and designed to enhance the streetscape and architecture.

Street Lighting



Pedestrian Lighting



### STREETSCAPE: REGIONAL ROAD 20 (HWY 20)

- Roadway Classification: Regional;
- Pedestrian walkways or/and Vehicular traffic;
- High Pedestrian/vehicle conflict/impact;
- Regional Road 20 (Hwy 20) is to be illuminated in a uniform way through their entire length, according to industry recommended standards;
- Luminaire type supports vehicular traffic as well as providing pedestrian-scale lighting for adjacent walkways;
- The illumination will dominate the nighttime environment, while the style of the fixtures will integrate into the character of the streetscape; and,
- Due to the illumination requirements for vehicular traffic, light standards must accommodate street lighting specifically.

### STREETSCAPE: RICE ROAD (RR54)

- Roadway Classification: Regional;
- Pedestrian walkways with multi-lane, high speed Vehicular traffic.;
- High Pedestrian/vehicle conflict/impact;
- Rice Road (RR54) is to be illuminated in a uniform way through their entire length, according to industry recommended standards;
- Luminaire type supports vehicular traffic as well as providing pedestrian-scale lighting for adjacent walkways;

- The illumination will dominate the nighttime environment, while the style of the fixtures will integrate into the character of the streetscape; and,
- Due to the illumination requirements for vehicular traffic, light standards must accommodate street lighting specifically.

### STREETSCAPE: CEREMONIAL ROUTE

- Roadway Classification: Collector;
- Pedestrian walkways with multi-lane, high speed Vehicular traffic;
- Medium Pedestrian/vehicle conflict/impact; and,
- Due to the illumination requirements for vehicular traffic, light standards must accommodate street lighting specifically.

### STREETSCAPE: MAIN STREET (WELLSPRING WAY)

- Roadway Classification: Collector;
- Pedestrian walkways with multi-lane, high speed Vehicular traffic;
- Medium Pedestrian/vehicle conflict/impact; and,
- Due to the illumination requirements for vehicular traffic, light standards must accommodate street lighting specifically.

### STREETSCAPE: COLLECTOR STREET C

- Roadway Classification: Collector;
- Pedestrian walkway with multi-lane, high speed Vehicular traffic;
- Medium Pedestrian/vehicle conflict/impact; and,
- Due to the illumination requirements for vehicular traffic, light standards must accommodate street lighting specifically.

### GATEWAY POND AND SOUTH POND

- Pedestrian walkways with multi-lane, high speed Vehicular traffic;
- Low Pedestrian/vehicle conflict/impact;
- Illumination levels are low and in compliance with industry recommended practices but must meet CPTED principles;
- Light-standards: Emphasis on Pedestrian character and scale. It desirable to maintain a uniform appearance of lighting equipment in most pedestrian walkways; and,
- The lighting will be uniformly distributed along the pedestrian walkway, creating a natural connection with urban spaces.

### MAIN URBAN SQUARE

- Pedestrian walkways with single-lane service driveways, speed-restricted Vehicular traffic;
- High Pedestrian/vehicle conflict/impact;
- The Urban Square will be the junction for pedestrian gatherings. It is illuminated to attract and engage pedestrians during various times of the night. Lighting allows for seasonal and themed programming for various cultural activities throughout the year;
- The lighting adds drama and excitement along the pedestrian square, creating a natural connection between activities and urban spaces;
- All streets terminating on Urban Square should be marked with two distinctly designed light-standard of equal size, signaling a pedestrian entrance points to the Square; and,
- Lighting to meet CPTED principles.

### Trail Lighting



### Urban Square Lighting

